

Volume 2: Goals, Policies & Actions

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ISSUES & OPPORTUNITIES



The land use element integrates ideas explored in the other elements to help make decisions about future community development. This promotes efficient, sustainable use of land by coordinating the size and location of future public facilities with the location and intensity of other future development types.

The characteristics of the City's land use are described in Volume 1 of this Plan. Land use planning is critical as it provides guidance for the most logical and efficient long-range land use pattern in the community.

The goals, policies and actions in this chapter are based on input from the public, City staff and various City committees and elected officials. The chapter begins with a summary of critical issues and opportunities. Several of the policies and actions are bolded to signify high priority.

The planning process includes a review of data that describe current conditions related to existing land use (See Volume 1) and many forms of input from stakeholders throughout the community. The following issues and opportunities stand out as most relevant to the goals, policies and actions in this chapter.

Balanced Growth



The Sun Prairie population has increased by more than 16% in the past 8 years, from 29,364 in 2010 to an estimate of 33,966 as of January 1, 2018 (about 4,600 people). With 500 new housing units permitted in 2018, the City is approaching 20% growth since 2010. For context, consider that the City grew by nearly 27% from 2000-2008, a period that included a nationwide housing boom and 400+ new units in Sun Prairie each year between 2000-2006.

There are about 70% more people calling Sun Prairie home today than in the year 2000.

The 2018 community survey shows that a majority of respondents are comfortable with this growth. For every land use except “multi-family residential”, most people felt that the pace of development was either “just right” or “too slow”, but the minority is vocal. Where given the opportunity to add written comments, the word “growth” showed up 273 times, and almost always in a critical way.

An important dynamic of community growth is the linkage between housing and employment. As described in the Economic Development chapter, unemployment is very low in 2018/2019 and business growth is constrained by a lack of workforce. Survey results favoring more growth in industrial and office uses and opposing residential growth reflect a poor understanding of how these uses are interrelated. Most glaring is the mismatch between the desire for industrial growth and opposition to multi-family housing.

Cities have limited means to control growth - regional market forces drive growth or decline. Sun Prairie is in a fast-growing region, has excellent highway access, and has space to grow. Property owners are responding to market forces when they seek annexation and development permits. The City's main focus has been on achieving quality development as projects are proposed, with requirements to ensure that traffic, stormwater and other utility needs are adequately met. The City has not attempted to limit growth with annual quotas on new permits for a variety of reasons, recognizing the rights of property owners to respond to market forces. There has also been concern that such policies could have unintended consequences, such as an increase in development costs and housing prices.

As described in the Housing chapter, the City is seeking healthy, balanced neighborhoods that feature a mix of housing types and price points. Balanced neighborhoods also tend to have convenient access to some form of smaller-scale commercial uses, such as a restaurant or coffee shop, day care facilities, small service businesses, etc.

Balanced Neighborhoods



Some Sun Prairie neighborhoods were explicitly designed to be complete neighborhoods with a mix of housing types, limited commercial use, park space, and completed sidewalk networks. Examples include Liberty Square (north side, west of Bird St.), Providence (west side along Hoepker Road), and Smith's Crossing (south side, east of USH 151). Other neighborhoods are unbalanced in various ways. Older neighborhoods, especially those built between 1950 and 2000, are often missing safe pedestrian facilities.

Many subdivision plats were designed with only low-density, single-family housing, and in some parts of the city there are large areas with very little residential diversity. There are a few areas, such as around Westside Elementary School, where the supply of park and open space and recreation improvements are low as compared to other neighborhoods, especially on a per-household basis.

One particular risk with unbalanced neighborhoods is the stress it places on the education system. When neighborhoods lack a mix of housing types they can become concentrated with either higher- or lower-income households. Because elementary schools tend to be smaller and more neighborhood-focused, significant socioeconomic differences between neighborhoods can result in significant differences in educational experiences and outcomes within neighborhood-based schools. This can be addressed with bussing, though the best outcome is to have complete neighborhoods with fewer built-in equity problems.

VOICES FROM THE COMMUNITY

Good Neighborhoods

The 2018 Community Survey asked an open-ended question about what the top three factors are that make a good neighborhood. Top responses included:

- Safety
- Good parks (nearby)
- Walkable
- Good schools (nearby)
- Controlled traffic

Growth

The 2018 Community Survey asked people if they believe the pace of development over the last 5 years in Sun Prairie is too fast, just right or too slow for certain land uses. Seventy-six percent (76%) of responses noted multi-family residential growth was too fast. Just over half of respondents noted that industrial (60%), office (61%), retail (56%) and 1- & 2- family residential (54%) growth was just right.

Redevelopment



As the City grows and changes, some of this change involves redevelopment of existing sites. This is a desirable process that the City is often actively encouraging. Infill development typically replaces a low-value building, often vacant, with new value and activity, boosting the tax base and the appearance of the area. These projects are often controversial because they are replacing a single-story building with multi-story development. Such outcomes are partly due to market forces – redevelopment is more complicated and expensive than development of farmland and the additional building area generates enough revenue to cover those higher costs. The larger buildings are also often an intentional outcome desired by the City, to make more efficient use of utility infrastructure – the greater concentration of taxable value provides more support to sustain the associated infrastructure through future decades of use and maintenance.

A central challenge with infill development is balancing the benefits of that development to the property owner and the City with the risk that the development has real or perceived negative impacts on existing, nearby development. Some portion of the negative reaction to infill is a reaction to change – many people simply prefer to see things stay more or less the same over time. But sometimes there are genuine negative impacts due to traffic or increased noise or exterior lighting.

Urban Design Guidance



Another aspect of healthy neighborhoods is home design that encourages social interactions. A hallmark of new-urbanist neighborhoods is the front porch. The front porch is a space that invites interaction, especially when the home is close to the street. More importantly, front porches can draw visual attention away from less attractive garage doors and present a more attractive streetscape. The least social home design puts the garage nearest the street and hides the front door much further back from the street. These designs typically have few windows facing the street, discouraging interaction between neighbors.



A similar principle applies in commercial districts. For decades after the post-war explosion in automobile ownership, commercial buildings were set back behind parking lots. As the buildings moved further from the street, and got larger, they often featured fewer windows. The net result of these changes has been places with much weaker connection between the private realm inside the building and the public realm of the street. In recent years local plans and national trends have helped move commercial buildings closer to the street, making the street corridor more attractive and lively and making the activities in the buildings more visible and inviting.

The 2009 comprehensive plan offered quite a bit of analysis of residential density. Though a popular metric for growth management, density is a poor indicator of neighborhood quality. The market viability and perceived quality of a neighborhood are affected more by building and site design than by density.

Retail Volatility



A common dilemma in many communities is how to deal with large-format retail stores. They present challenges for urban design because of their large parking lots and lack of visual connection between the building and the street. They present challenges to other local retailers, threatening their viability in the local market. And they present challenges for reuse and redevelopment when they close. Many Wisconsin retailers have recently been reducing their tax payments by arguing, successfully, that their buildings should be valued based only on what they are worth when empty. The problem is that unlike an empty house, a large, empty store has very few potential buyers. The “dark” stores are worth less because the market for them is poor. There are two problems here: One is that when the assessed values of large-format retail stores are reduced while they are still operating, it shifts more property taxes onto all other properties in the city. The other is the poor resale value (and sometimes extended vacancy) of these buildings. Changes in the retail industry, especially pressure from online sellers such as Amazon, are a constant threat to the viability of retailers of any size. Many of the large retail stores in Sun Prairie have been constructed within the past five years, suggesting that the near-term risk of empty stores is low. However, as those stores age, and as national and regional trends continue to evolve, the City will someday need to find replacement uses for some of those big buildings.

Automobile Dependency



As described in the Transportation chapter, the City intends to improve transportation options for residents, including bike and pedestrian facilities and public transit service. These objectives are challenged by 75 years of land development designed around the automobile. Most of the residential and retail areas built since 1945 were intentionally separated from each other (into distinct zones, thus “zoning”) and featured 1-2 story buildings with plentiful off-street, surface parking. Only within the past 20 years have development practices shifted back toward balanced neighborhoods in which walking is both safe and viable as a means of transportation. Smith’s Crossing, Liberty Square and Providence are local examples of new neighborhoods in which walking is a viable means of transportation, at least for a few commercial destinations, because of sidewalks, more compact housing formats that reduce walking distances, and the presence of commercial in the neighborhood.

Transit service is most likely to thrive where land use is most efficient. When lots of people can walk to a transit stop, ridership increases, which leads to more frequent service, which draws more people to use transit. Current efforts toward fixed-route transit service in Sun Prairie are focused on express service from a park and ride location in Sun Prairie to Madison. Gathering transit riders at park and rides will always be part of the transportation system. However, a more robust transit system in Sun Prairie is partly dependent on the development of places and corridors where mixed-use, multi-story development is prevalent, putting more people close to transit.

GOALS, POLICIES & ACTIONS



Westside Sun Prairie Growth Pattern: 2000 (left) to 2017 (right)

GOAL #1

Create an economically and environmentally sustainable development pattern.

Policies

1. **New development within the City's jurisdiction should support the efficient use of public services and infrastructure.**

- a. Land development will be coordinated with facilities and services planning.
- b. Infill development that uses existing infrastructure is strongly encouraged. Leap-frog development that requires costly infrastructure extensions through undeveloped lands is discouraged.
- c. All non-agricultural development within the Sun Prairie Urban Service Area (USA) shall be served with the com-

plete array of City services including sanitary sewer, storm sewer, municipal water, City police protection, fire protection, etc. The City will not approve proposals that would result in the creation of unsewered urban development within the Sun Prairie USA.

d. The City of Sun Prairie shall not extend its sanitary sewer and municipal water lines to development that is not located within the City's corporate limits.

e. All lands located within the Sun Prairie USA will eventually be located within the City's corporate limits.

f. The City of Sun Prairie will strongly oppose any proposal to create a public sewer or water utility system in the neighboring townships that is intended to compete with the City's municipal systems, to support urban develop-

ment densities in unincorporated areas, or to infringe in any way on the City's ability to expand its utility systems and municipal boundaries in a logical and efficient manner.

2. **Encourage development that uses land efficiently, including:**



- a. Infill development and higher density redevelopment on sites already surrounded by urban uses.
 - b. Residential formats in every neighborhood that minimize land consumption, including small lots and attached units.
3. Strive for balanced growth such that increased employment opportunities are matched with increased residential options.



Indicates a policy or action that supports community sustainability

Policies and actions in **bold text** indicate high priority

4. Enable and encourage well-planned, mixed-use development areas in all parts of the City, including the integration of small-scale commercial uses into neighborhoods and mixed-use buildings where economically viable.
5. Strongly discourage rural development in the City's Urban Reserve areas that would impede future City growth.
6. The City will not prematurely zone property for development prior to a specific development proposal.
7. Encourage development formats and building designs that support adaptive reuse as markets shift and demand changes. While unique building design is encouraged, designs that render the building hard to use for other occupants in the future are discouraged. Large-format commercial buildings should be reasonably divisible into multi-tenant buildings.
8. Encourage landscaping practices in new and existing neighborhoods that protect ground water and surface water resources.
9. Use the New Neighborhood Staging Plan in this chapter to guide decisions for new neighborhoods when reviewing annexation petitions, urban service area amendments requests, zoning map amendment requests, land division proposals.

Action

1. Pursue and advocate to WisDOT for a new interchange at US 151 and CTH V V, connecting to an extended Egge Road

east from N. Bristol Street, which would improve access to the Sun Prairie Business Park. Prior to construction of this interchange, develop a detailed area plan that considers the desire for high-intensity development adjacent to the Sun Prairie Business Park that can support jobs and a potential Bus Rapid Transit link to Madison. Also consider the potential for a sports complex to support recreation for City residents (should there still be demand for such a facility).

GOAL #2

Support personal mobility for all residents in all development and redevelopment projects.

Policies

10. All new development will be designed for safe walking and biking including:

- a. Consistency with the City's sidewalk policy.
 - b. Direct and convenient pedestrian routes separated from vehicle routes, including connections from the sidewalk to building entries.
 - c. Designated bike parking.
 - d. Designated bike paths per transit map.
11. All development or redevelopment along arterial streets shall consider the potential for transit service and dedicate right-of-way accordingly.

12. When planning redevelopment and new development along major arterial streets, consider Transit Oriented Development (TOD) strategies, even if regional transit service is not yet

robust. TOD areas will have a mix of uses including commercial and high-density residential uses and can improve the sustainability and frequency of transit service.

13. Protect the existing freight rail line for short term freight use and long term regional transit use.

GOAL #3

Establish balanced neighborhoods throughout the City.

Policies

14. Provide a mix of housing types to accommodate every stage of life in every neighborhood (see also Housing, Vol. 2 Chapter 7).

15. Use site plan, landscaping plan, and building design review processes to encourage design for land use compatibility. Consider the following design guidelines in these reviews:

- a. Maintain adequate physical separation between residential areas and uses that tend to produce excessive noise or odors (land use planning), without sacrificing pedestrian and bicycle connectivity.
- b. Use screening (walls, landscaping, berms) to create separation when distance alone is not possible or sufficient (site planning).
- c. Design the site so that major activity areas such as building entrances, service and loading areas, parking lots and trash receptacle stor-

age areas are oriented away from less intensive land uses to the greatest degree possible.

d. Reduce the impact of parking areas with physical separation and visual barriers (site design).

e. Direct traffic from higher-volume uses to collector and arterial streets, away from neighborhood streets (land use planning, site design).

f. Require basic landscaping, building design and materials standards for all multi-family uses and nonresidential uses in neighborhood settings. The City should adopt design standards into the zoning ordinance to achieve this.

g. Encourage building and site design techniques to make larger buildings more compatible with smaller residential uses, such as increased setbacks, stepped-back upper stories, and architectural strategies to break up the apparent volume of the building.

h. Outdoor lighting of parking, storage and service areas shall be designed to minimize spillover of light onto adjacent properties and public rights-of-way.

i. Encourage underground parking facilities when possible/feasible.

16. Locate community facilities such as schools, churches, libraries, museums, parks and community centers in strategic locations that enhance and are safely accessible from the surrounding neighborhoods.

17. Institutional land uses designated by the Institutional and Utilities category on the Future Land Use Map (schools, churches, community centers) should communicate with surrounding neighbors whenever they are considering long-term expansion plans. City staff should facilitate this process by creating (or requiring) detailed small area plans that include the possible expansion.

18. Promote quality neighborhood development that includes an appropriate mix of uses by requiring the adoption of detailed neighborhood plans prior to development approvals, unless detailed land use plans are adopted as a component of this plan. Neighborhood plans should address compatibility with adjacent land uses, transportation connectivity, and provision of various city services and utilities. Neighborhood plans should be adopted into this Comprehensive Plan, including revisions to the future land use maps as appropriate. Require consistency with this Comprehensive Plan in all zoning and land division decisions.

19. Encourage building and neighborhood design to enable passive solar heating and photovoltaic power generation.

a. Either require bike parking spaces in new developments or encourage bike parking by offering a reduction in vehicle parking.

b. Amend the institutional land use regulations to require a detailed small area plan and a neighborhood meeting prior to submitting a formal development application for City review.

c. Enable the installation of solar energy systems in most zoning districts and streamline their approval.

d. Consider the use of density bonuses as an incentive to encourage enhancements that the City wouldn't require, such as energy and resource efficient design or income-qualified housing units.

e. Consider the adoption of **form-based overlay zoning districts** for planned mixed-use areas that provide more detailed building and site design requirements than the underlying zoning districts, including designation of critical build-to lines and building massing requirements. Update existing standards for the Main St. Overlay.

f. Review and strengthen landscaping, signage, lighting and building design standards in the City's Zoning Ordinance, including standards for employment areas, commercial uses, multi-unit housing and along key corridors and community entries. Maintain basic design standards for buildings and landscaping to protect aesthetic



Actions

2. Complete a comprehensive update and unification of the Zoning and Subdivision Ordinances, including the following changes:



Indicates a policy or action that supports community sustainability

Policies and actions in **bold text** indicate high priority

quality within neighborhoods. Consider the design guidance offered within each of the land use areas described on the future land use map in this Comprehensive Plan. Design guidelines should address the following items (whenever applicable to the use):

- i. Relationship of the building and other site features to the street
 - ii. Standards for building placement on corner lots
 - iii. Location and screening of parking, loading areas, dumpsters, and utility equipment (roof top and site)
 - iv. Location and canopy design for drive-through facilities, gas pumps, service bays, and car washes
 - v. Treatment of outdoor display and sales areas
 - vi. Building architecture and materials, including the use of windows
 - vii. Landscaping, signage, and lighting
 - viii. Pedestrian and vehicle access and circulation; both on-site and between the site, as well as adjacent sites and the street
 - ix. Parking lot location, design and layout
- g. Amend the subdivision and other development ordinances to address high groundwater issues associated with development, particularly with respect to buildings with basements.
3. Develop neighborhood area plans for the South and East Planning Areas for lands identified as Urban Reserve on the Future Land Use Map. The plan(s) shall be developed and

WHAT IS FORM-BASED ZONING?

Form-based zoning is defined by the Form Based Codes Institute as:

A method of regulating development to achieve a specific urban form.

Form-based codes create a predictable public realm primarily by controlling physical form, with a lesser focus on land use.

Form-based codes address the relationship between building facades and the street, the form and size of buildings in relation to one another, and the scale and types of streets and blocks. The regulations and standards in form-based codes, presented in both diagrams and words, are keyed to a regulating plan that designates the appropriate form and scale (and therefore, character) of development rather than only distinctions in land-use types. Form-based codes are regulatory, not advisory.

adopted prior to construction of major utility projects that will spur urban development in these areas (e.g., Far Eastside Sewer Interceptor).

4. Enforce property maintenance codes to maintain neighborhood quality and prevent blight.

GOAL #4

Support the land use needs for employment growth.

Policies

20. Protect lands to ensure their availability for the continued expansion of the Sun Prairie Business Park, with allowances for other uses that complement the employment center, including commercial and residential clusters.
21. Maintain a supply of lands identified in the future land use map for more intensive industrial uses that are not a good fit for the Sun Prairie Business Park, including any uses with heavy water and sewer demand. Most of this should be in the Bailey Road corridor near the water pollution control facility.

22. Ensure that there are spaces and lands available to accommodate new office uses in commercial, employment and mixed-use areas.

23. Enhance the function and aesthetic quality of business districts by adopting and enforcing high-quality design standards in the City's Zoning Ordinance.

a. Outdoor storage and loading areas shall be screened from the view of public rights-of-way and adjacent properties. Garage doors and parking lots should not dominate the site as viewed from the right-of-way.

b. Signage shall be of high quality design and shall not be excessive in total area and height.

c. Industrial developments shall be designed so that truck loading and staging and movement from one part of the site to another can occur without use of a public street.

GOAL #5

Support well-designed and sustainable retail and mixed-use development.


Policies

24. Encourage the development of compact, carefully planned, mixed-use activity centers that include shopping, employment, housing, recreation, and community gathering opportunities. Mixed-use buildings and developments that include residential units with commercial uses are desirable and often necessary to achieve re-development.

25. Continue City efforts toward strengthening and enhancing the downtown.

26. Monitor and study downtown parking supply and demand to seek efficient use of parking investments and a good balance between total cost and user satisfaction. Consider signage, pricing and management interventions to maximize efficiency.
27. Identify commercial development opportunities in neighborhood settings that fit the site, both in terms of specific use and building and site design.
28. Discourage the development of uncoordinated strip commercial land uses.
29. Encourage infill development of vacant or under-utilized lands or buildings, especially

along high-profile corridors. Work with property owners along W. Main Street to bring forward projects consistent with this Comprehensive Plan and the Central Main Street Plan.

30. Encourage continued use, maintenance and adaptive reuse of existing buildings with historic value. 
31. Encourage infill development consistent with Main/Bristol Plan.
32. Encourage strong public-private investments in business district improvements and facilities. The City recognizes that strong public-private partnerships are often the key to successful redevelopment efforts, and shall work to establish such relationships, including the use of Tax Incremental Financing as feasible and appropriate.
33. Development incentives such as waivers, flexibility, and/or TIF assistance should be focused on infill and redevelopment, not greenfield development at the edge of the City.
34. Work to achieve a balance among various commercial areas with design formats, placemaking amenities, and business mixes that allow each area to compete successfully and sustainably in the market.

Action

5. Update the Downtown Revitalization Plan.
When updating the Downtown

Plan, consider for inclusion the following preferences identified by the Business Improvement District (BID):

- a. More opportunities for sidewalk café/restaurant seating, including bump-outs.
- b. High-quality enclosures for trash and recycling containers, including walls and roofs.
- c. Enhanced signage guidelines and regulation, including limits on window signs and sign lighting, encouragements for projecting signs visible along the sidewalk, and sign-removal requirements when businesses leave.
- d. Improved lighting for pedestrian areas, to make sidewalks safer and more attractive.
- e. Encourage property owners that start construction or exterior renovation projects to complete them promptly.
- f. Make the downtown bike-friendly, including more bike racks.

GOAL #6

Create places that are attractive and unique.

Policies

35. Encourage and facilitate placemaking strategies to make neighborhoods and commercial areas unique, memorable, and attractive. These strategies can be implemented quickly during new development or incrementally over time in existing developed areas. The Project for Public Spaces is a great resource for



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Policies and actions in **bold text** indicate high priority

ideas during the development process. Some examples of place-making strategies:

- a. Incorporate squares and parks as multi-use destinations.
- b. Design buildings to activate outdoor spaces with entrances and ample windows, café space with outdoor seating, etc.
- c. Incorporate art into infrastructure and outdoor spaces
- d. Incorporate recreation into all public spaces.
- e. Design with priority to walking and biking.
- f. Allow for experimentations and change. Places may need to be adapted to changing needs and conditions.

- 36. Enforce landscaping requirements for all development and encourage preservation of mature trees whenever possible and appropriate to the site.
- 37. Encourage the preservation of architecturally, historically, and culturally significant sites, buildings and structures in the City and its environs.

Action

- 6. Consider the creation of a Design Review Board.

GOAL #7

Protect and respect natural resources and systems in all development decisions.

Policies

38. Limit development on lands with poor soils and high groundwater to help minimize damage to

SOIL SUITABILITY FOR DEVELOPMENT

Some neighborhoods within Sun Prairie have experienced issues with high groundwater and seepage into basements after major rains. At the time these neighborhoods were constructed, groundwater levels and soil types were not a major factor used to determine where new neighborhoods should be located. Figure 9-13 in the Land Use chapter of Volume 1 identifies depth to groundwater. Areas where groundwater is close to the surface may not be appropriate for development with basements.

property and groundwater recharge areas.

39. Protect and enhance Patrick Marsh and the lands surrounding this natural resource by acquiring the land and/or development rights and improving it for recreation access.

40. Preserve environmental corridor features including waterways, floodplains, wetlands, ground water recharge areas, steep slopes (greater than 12%), wildlife habitat, scenic vistas, and mature woodlands through the enforcement of the City's adopted natural resource protection zoning standards and through the use of the City's Subdivision Ordinance, Zoning Ordinance and Official Map.

41. Continue to honor the greenspace preservation provisions of the City's Cooperative Plan with the Town of Burke, Village of DeForest and City of Madison.

42. Foster the protection of natural resource features from development by requiring those features to be accurately depicted on all site plans, certified survey maps, preliminary plats, and final plats.

43. Encourage the clean-up of contaminated sites to foster redevelopment of brownfields.



44. Locate transportation system improvements to minimize impacts on environmental corridors.

45. Consider the suitability of lands for buildings with basements when making decisions regarding future growth and development.

46. Protect the Token Creek Watershed as northern and western portions of the City develop.

Action

7. Work closely with the Capital Area Regional Planning Commission to protect water resources while ensuring an adequate supply of land in the urban service area to accommodate growth as anticipated in this plan.

GOAL #8

Partner with neighboring towns, cities, and villages to achieve sustainable development patterns.

Policies

47. Continue to implement the Town of Burke, Village of DeForest, City of Sun Prairie and City of Madison Cooperative Plan; the City of Sun Prairie and Town of Bristol Intergovernmental Agreement; and the City of Sun Prairie – Village of Windsor Intermunicipal Agreement, including commitments to communication and collaborative planning.
48. Work with neighboring communities to foster an orderly, efficient and sustainable development pattern that reserves land for future transportation corridors, preserves natural resources and minimizes conflicts between urban and rural land uses.
49. Seek an intergovernmental agreement with the Town of Sun Prairie in order to avoid municipal boundary disputes.
50. Whenever possible, coordinate the plans of federal, state, regional, county and local governments with the plans of the City of Sun Prairie.
51. Use the annexation and attachment policy (outlined below) to react to annexation/attachment petitions.

ANNEXATION & ATTACHMENT POLICY

The future growth and development of the City will likely involve a combination of redevelopment and infill development within the existing developed area of the City, and new growth at the edge of the community. While there are approximately 870 acres of undeveloped land already located within the corporate limits, over time there will be a need to incorporate additional lands through annexation or attachment to accommodate urban expansion or to capitalize on desired development opportunities. The policies contained herein should be used to guide such decisions as urban expansion through annexation or attachment is proposed.

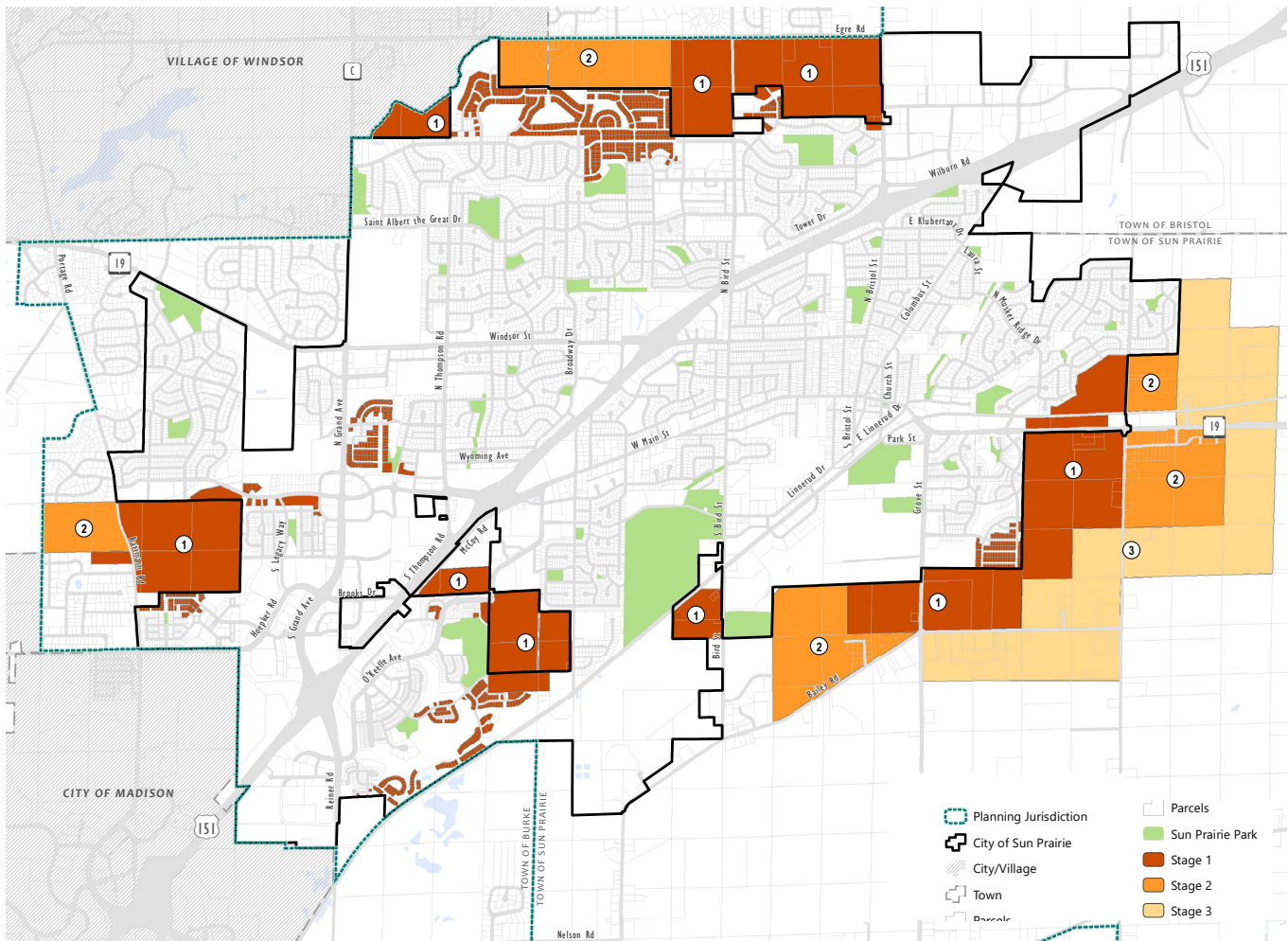
- A. The City encourages annexations and attachments to occur prior to urban development to ensure that such development is consistent with the City's Comprehensive Plan, Zoning Ordinance, Subdivision Ordinance, Official Map and Capital Improvements Program.
- B. The City of Sun Prairie will not provide access to its municipal utility systems to lands that are located outside the City's corporate limits.
- C. The City of Sun Prairie encourages cooperative intergovernmental planning with neighboring communities to avoid inefficient development patterns and intergovernmental boundary conflicts. Existing agreements shall be honored, and shall be reviewed and updated/renegotiated periodically as needed. New agreements should be pursued with neighboring communities where they currently do not exist, particularly with the Town of Sun Prairie.
- D. The City shall require detailed concept plans associated with any annexation or attachment proposal that involves the development or redevelopment of the lands involved.
- E. Lands annexed or attached to the City shall be temporarily placed in the most restrictive zoning classification, based on property size, until a plat or development proposal is approved for the property.
- F. The City may require a pre-annexation agreement to outline the expectations and parameters associated with the extension of infrastructure or other development or redevelopment issues associated with the lands proposed for annexation or attachment.
- G. Attachments to the City will be made in accordance with the final Town of Burke, Village of DeForest, City of Sun Prairie and City of Madison Cooperative Plan or any applicable future agreements made between the City and adjacent communities.
- H. The City Planning Commission and Common Council shall consider, at a minimum, the following factors when reviewing proposals for annexation or attachment to the City of Sun Prairie:
 - Does the area proposed for annexation or attachment have access to, or can it be easily connected to, areas already served by the City thereby allowing efficient delivery of services, facilities and utilities to the area?
 - Can the City readily provide water service, sewer service and other infrastructure to the area proposed for annexation at little or no cost to City taxpayers, or is there a significant public benefit associated with the incorporation of the lands involved if City expenditures are involved in extending such infrastructure?
 - Does the school district have the capacity to accommodate students who live or will live within the area proposed for annexation?

(continued on the next page)

ANNEXATION & ATTACHMENT POLICY (CONTINUED)

- Is the proposed development or redevelopment of the annexation or attachment area consistent with the policies and recommendations of this Comprehensive Plan, including the recommended land uses and development pattern illustrated on the Future Land Use Plan Map?
 - Is the proposed annexation consistent with the New Neighborhood Staging Plan (discussed on the next page) in terms of the desired timing and sequence of development in the City?
 - Is the proposal in keeping with market conditions, as measured by factors such as, but not limited to, vacancy/occupancy rates, inventory of vacant lots, number of active development projects in the City, etc., at the time annexation or attachment is being contemplated, and does there appear to be a need for the incorporation of additional lands to accommodate the land uses being contemplated at the time of annexation or attachment?
 - Are there public benefits associated with the proposed annexation or attachment (fiscal, economic development, land use, infrastructure connectivity, control of land in key growth areas, etc.) to sufficiently offset any negative aspects that may be associated with the proposal?
- I. In situations where 1) detailed City plans do not exist, 2) proposed development does not meet the New Neighborhood Staging Plan timeline, or 3) significant changes to a City plan are proposed, the petitioner(s) for annexation shall be required to provide the City with a detailed fiscal impact analysis using standards defined by the City that specifies the expected impacts of existing and proposed development on the property proposed for annexation on the City's tax rate and its ability to provide efficient, sustainable levels of service for all essential City services, facilities and utilities, before an annexation ordinance will be considered for adoption by the City.

Figure 9-1: New Neighborhood Staging Map (See Map 9-14 in Appendix D)



NEW NEIGHBORHOOD STAGING PLAN

As the City of Sun Prairie continues to grow, it is important to manage the pace and location of that growth so that new development can be adequately and efficiently served with various public services. The New Neighborhood Staging Plan Map (see **Figure 9-1** on the previous page) prioritizes neighborhood growth areas to encourage the development of lands with good access to infrastructure and delay the development of other lands.

The Staging Plan is intended to serve as a general guide for the Plan Commission and Common Council when those bodies review annexation petitions, urban service area amendment requests, zoning map amendment requests, land division proposals, and when the City considers the location and timing of capital improvement projects. Changes in any of the following factors will influence the timing and location of future City expansion into the planned development staging areas in and around the City of Sun Prairie:

- The City's ability to serve the respective areas with the full range of urban services, facilities and utilities without undue strain on these services
- The landowner's interest and willingness to develop their properties in accordance with the adopted land use and/or neighborhood plans for their areas
- The degree to which annexation petitions are consistent with the City's annexation policies and criteria
- The need for and ability to obtain urban service area amendments
- The suitability of lands for various types of urban development
- The availability of transportation improvements necessary to serve the area
- Real estate market conditions in and around the City of Sun Prairie, including the number of available lots and/or dwelling units already approved and available for development in the City

Not all areas depicted on the Staging Plan Map are expected to develop within the next twenty years. In fact, it is likely that many of the planned development areas will remain undeveloped well beyond the year 2040. Nevertheless, all future City development areas that are depicted on the Staging Plan Map are expected to be located within the City of Sun Prairie's corporate limits at some time in the future.

This Staging Plan does not apply to infill development on sites with ready access to public utilities.

Stage 1 (Ready for Development)

Areas on the map labeled "Stage 1" include platted neighborhoods with lots still available for construction and lands that are not yet platted or approved for development but which can be easily served with public utilities. The City would like to see these lands develop first.

Stage 2

Areas identified on the map as "Stage 2" are generally either not readily served with utilities and/or not contiguous to an existing City neighborhood as of 2019. These areas may require the construction of new or expanded facilities or utilities (such as wastewater lift stations or looped water lines) before development can occur, or are lands where development offers less public benefit than Stage 1 development. Stage 2 lands should not be developed until about one-half of the Stage 1 land area is platted and at least fifty percent improved and the land is contiguous to existing City development.

Stage 3

Areas designated as "Stage 3" are generally those lands where more extensive facilities, service or utilities (e.g. new interceptor sewers, water towers, major streets etc.) are needed to accommodate urban development as of 2019. Stage 3 lands should not be developed until about one-half of the Stage 2 land area is platted and at least partially improved and the land is contiguous to existing City development.

Development in portions of this area that would require a new lift station in order to have sewer service should be discouraged and delayed until the Far East Sewer Service Interceptor is created to serve those lands.

FUTURE LAND USE GUIDE



Construction of MacDon Enterprises' New Facility in Business Park

The Future Land Use map and policies incorporates ideas from throughout all preceding sections of this plan, including the Volume 1 background data and the Volume 2 policy chapters on other topics.

USING THE FUTURE LAND USE MAP

The Future Land Use Map contains different land use categories that together illustrate the City's land use vision. These categories, including explanation of the City's intent, zoning, design and development strategies for each, are described in this chapter.

This plan includes one City-wide Future Land Use Map along with several "Planning Area" maps. Planning area maps are scaled to enable more detailed illustrations of the locations throughout the City where residential, mixed-use, commercial, industrial, public, open and park spaces should be located. The Planning Area maps

also indicate high-priority redevelopment sites.

The Future Land Use Map presents recommended future land uses for the City of Sun Prairie and its extraterritorial jurisdiction. This map and the associated policies forms the basis for land development decisions and are to be consulted whenever development is proposed, especially when a zoning change or land division is requested. Zoning changes and development shall be consistent with the future land use category shown on the map and the corresponding plan text.

Statement of Intent & Typical Use

The future land use categories identify areas by their primary intended uses, character and densities (herein described as "Statement of Intent & Typical Uses"). These classifications are not zoning districts - they do not establish binding performance criteria for

land uses (i.e. setbacks, height restrictions, etc.) nor are they intended to list every possible use that may be permitted within the future land use class classification.

Parcels on the Future Land Use Map are identified by their primary intended uses; however, some of the parcels on the Future Land Use Map have yet to be platted or subdivided. The City recognizes that detailed site planning to identify precisely how larger unplatted parent parcels (herein referred to as "unplatted new development areas") may be subdivided, zoned and developed is outside of the scope of a comprehensive plan. The City may create neighborhood plans for these areas as part of future amendments to this Comprehensive Plan to further illustrate and guide development within these areas. The City may also require that developers create neighborhood plans for these areas prior to submitting requests for rezonings or preliminary plats.

Potentially Acceptable Zoning Districts

The future land use classifications identify those existing City of Sun Prairie Zoning Districts that are “consistent” within each future land use category (herein described as “Potentially Acceptable Zoning Districts”). The list of potentially acceptable zoning districts will be used by the City to confirm whether requests for rezoning of property are generally consistent with this plan. Areas subject to City zoning districts include all lands located within the City of Sun Prairie municipal boundary. Areas outside the municipal boundary, but within the City’s extraterritorial plat review jurisdiction are subject to the City’s Subdivision Ordinance in addition to applicable county or local township zoning and land division regulations.

Effect on Zoning

Land use and design policies in this plan should be considered during all development processes, especially in land division and rezoning or zoning ordinance amendment processes when consistency with the plan is a statutory requirement. Where development is proposed under existing zoning regulations, including any Planned Development districts, the regulations of existing zoning supersede policies in this plan.

Best Practice Design Strategies

The Best Practice Design Strategies listed within each category are provided to help developers and City officials make design decisions during the development process consistent with the intent of the future land use category and the general desire for high quality site and building design. These strategies may be used to help

determine whether to approve rezoning, conditional use permit, site plan, or planned unit developments. The illustrations and photos are not an exhaustive list of best planning practice and do not constitute the whole means by which high quality site and building design can occur.

The identification of future land use categories and potentially acceptable zoning districts does not compel the City to approve development or rezoning petitions consistent with the future land use category or map. Other factors will have to be considered, such as the quality of the proposed development, its potential effect on adjacent properties, its potential effect on City transportation infrastructure, the capacity and ability to provide services to the site, and the phasing of development, before any development applications are approved. In addition, it is not anticipated that all areas suggested for future (re)development on the Future Land Use Map will develop or be rezoned for development immediately following adoption of this Comprehensive Plan. In some cases it may be years or decades before (re)development envisioned in the plan occurs due to market conditions, property owner intentions, and City capacity to serve new (re)development.

Detailed Neighborhood Concepts

Based on what is known about the location and capacity of existing and planned utilities and other infrastructure, environmental characteristics, and approved yet not constructed development, potential city expansion areas have been identified to accommodate projected population growth in the City. The three main areas are

Northside/Egre Road, O’Keeffe Avenue/Reiner Road, and the Near Eastside. These three areas represent the places most appropriate for future development once approved developments and existing neighborhood plan areas are developed and built. These areas were determined during the 2009 comprehensive planning process and were found to still be valid in the current planning process.

Each of the three detailed planning areas has different characteristics that influence what type of development is most appropriate in each location. Characteristics of each planning area and conceptual development plans for each area are presented and described later in this chapter. Plans created for the area are based on existing conditions, existing and planned infrastructure, adjacent land uses, and public input. The plans are incorporated into the Comprehensive Plan as part of the Future Land Use Plan Map.

The Future Land Use Plan Map, including these detailed planning area plans, represents a conceptual development pattern to illustrate the desired vision for future neighborhoods and is based on community preferences and available information regarding the development limitations present in the City’s future growth areas. Future development on any particular property will be subject to a higher level review, and precise land use delineations and environmental features will be determined by on-site investigation and engineering at that time. This means that lines may shift and street patterns may be altered from those shown on the Future Land Use Plans when more accurate information is available. However, the general land use pattern and neighborhood

character represented on the Future Land Use Plan should be respected. See the Land Use Workshop Summary sidebar for more information supporting the type of land use in the three growth areas.

AMENDING THE FUTURE LAND USE MAP

It may from time to time be appropriate to consider amendments to the Future Land Use Map, usually in response to a type of development not originally envisioned for an area when this plan was adopted. See **Chapter 10** for a description of the procedural steps for amending any aspect of this plan. The following criteria should be considered before amending the map.

Compatibility - The proposed amendment/development will not have a substantial adverse effect upon adjacent property or the character of the area, with a particular emphasis on existing residential neighborhoods. A petitioner may indicate approaches that will minimize incompatibilities between uses.

Natural Resources - The land does not include natural features such as wetlands, floodplains, steep slopes, scenic vistas or mature woodlands (1 or more acres, especially those consisting of heritage trees), which will be adversely affected by the proposed amendment/development. Any proposed building envelopes are not located within the setback of Shoreland and Floodplain zones (or is raised above regional flood line). The proposed development will not result in undue water, air, light, noise pollution or soil erosion. Petitioners may indicate those approaches they intend to use to preserve or enhance

the most important and sensitive natural features of the proposed site and mitigate impacts to surrounding properties.

Transportation - The lay of the land will allow for construction of appropriate roads and/or driveways that are suitable for travel or access by emergency vehicles. The proposed amendment/development will not create a significant detriment to the condition of adjacent transportation facilities or cause significant safety concerns for motorists, bicyclists, or pedestrians. Petitioners may indicate those approaches they intend to use to mitigate transportation compatibility concerns.

Ability to Provide Services - Provision of public facilities and services will not place an unreasonable financial burden on the City. Petitioners may demonstrate to the City that the current level of services in the City, or region, including but not limited to school capacity, emergency services capacity (police, fire, EMS), parks and recreation, library services, and water and/or sewer services, are adequate to serve the proposed use. Petitioners may also demonstrate how they will assist the City with any shortcomings in public services or facilities.

Public Need - There is a clear public need for the proposed change or unanticipated circumstances have resulted in a need for the change. The proposed development is likely to have a positive social and fiscal impact on the City. The City may require that the property owner, or their agent, fund the preparation of a fiscal impact analysis by an independent professional.

Adherence to Other Portions of this Plan - The proposed amendment/development is consistent with the general vision for the City, and the other goals, policies and actions of this plan.

INTERPRETING BOUNDARIES

Where uncertainty exists as to the boundaries of future land use categories shown on the Future Land Use Map, the following rules will apply. If uncertainty still exists, the City Council shall decide any inquires related to map boundaries upon recommendation of the Plan Commission.

1. Boundaries indicated as approximately following the center lines of streets, highways, or alleys will be construed to follow such center lines.
2. Boundaries indicated as approximately following platted lot lines or U.S. Public Land Survey lines will be construed as following such lot lines.
3. Boundaries indicated as approximately following municipal boundaries will be construed as following such boundaries.
4. Boundaries indicated as following railroad lines will be construed to be midway between the main tracks.
5. Boundaries indicated as following shorelines and floodplains, will be construed to follow such shorelines and floodplains, and in the event of change in the shorelines and floodplains, it will be construed as moving the mapped boundary.
6. Boundaries indicated as following the center lines of streams, rivers, canals, or other bodies of water will be construed to follow such center lines.
7. Boundaries indicated as parallel to extension of features indicated above will be so construed. The scale of the map will determine distances not specifically indicated on the map.

FUTURE LAND USE PLAN



New Residence in Providence Subdivision

FUTURE LAND USE CATEGORIES

This section includes a description of each of the Future Land Use Plan categories. These categories include recommended land uses (e.g. residential, commercial, industrial), and land use densities (i.e. dwelling units per acre). All zoning decisions, land divisions, utility extensions, capital improvement projects, and related land development activities and decisions should be consistent with the recommendations of the Future Land Use Plan.

Refer to the Planning Area Maps of this chapter for more information regarding specific planning areas.

Categories:

NR

Neighborhood Residential

HIR

High-Intensity Residential Overlay

NMU

Neighborhood Mixed Use

CMU

Community Mixed Use

DMU

Downtown Mixed Use

GC

General Commercial

E

Employment

I

Industrial

IU

Institutional & Utilities

POS

Parks & Open Space

UR

Urban Reserve

RL

Rural Lands

NRP

Natural Resource Protection Overlay

NR Neighborhood Residential

NR areas provide a mix of housing types, civic uses (e.g., place of worship, social service clubs, etc.), and daycare facilities. Most of the area designated as **NR** is or will be used for single family homes, but a variety of other housing types are appropriate within this designation, including duplex, town home, and small multi-unit formats (up to 8-units in a building). More intensive multi-unit housing, i.e. buildings with more than 8 units, may also be compatible within **NR** areas. Specific locations for such housing are designated and policies are provided to guide their occasional use elsewhere in **NR** areas.

The purpose of the **NR** designation is to achieve neighborhoods with a mix of housing types while also ensuring compatibility between differing housing types and forms. The following policies include design guidelines to ensure compatibility.

1. **NR** areas will have both rental and owner-occupied housing, and provide housing options for people of all ages and physical abilities.
2. Housing will be one to three stories in height with residential densities in most places of 3-20 units per net acre (excluding streets, parks, outlots, etc.).
3. In new neighborhoods, the creation of a detailed neighborhood plan and/or Planned Development Zoning is strongly encouraged to identify specific locations for various housing types and densities.
4. In existing neighborhoods, the creation of a detailed neigh-

borhood plan is encouraged to provide site-specific guidance for any proposed infill or redevelopment.

5. When integrating housing forms other than single-family detached, whether in new or existing neighborhoods, the following policies should inform neighborhood design and/or infill redevelopment design and approval. As more detailed neighborhood plans are prepared and adopted for specific neighborhoods (either new or existing), additional site-specific designations in those plans may supersede these policies.

a. Accessory dwelling units should be permitted in any single family housing district. The zoning ordinance will be updated to include specific design standards for such units, including setback, height and area regulation, as well as provisions regarding owner-occupancy.



Carriage/Garage House



Detached Accessory Unit

b. Duplex units are appropriate just about anywhere within a neighborhood, as follows:

- i. On any corner lot, if each unit faces and is ad-

dressed to a separate street and meets the standard setback requirements and pattern typical along the street.

ii. In the middle of a block between single family detached homes, if substantially similar to other homes along the street in massing, architectural character, total garage doors, and driveway width.

iii. As a transitional use when facing or next to a more intensive institutional, residential or commercial use. In this case there should be some general consistency of form and style with other homes in the neighborhood, but also more flexibility in design as compared to sites surrounded by single family homes.



c. Townhomes or rowhouses with up to 6 contiguous units are appropriate in any neighborhood, as follows:

- i. When facing or adjacent to a commercial use, large institutional use, or residential use of equal or greater intensity.
- ii. When facing a public park or permanent green space.



d. Small multi-unit buildings with up to 8 units per building may be appropriate in any neighborhood, evaluated on a case-by-case basis, if ALL of the following apply:

- i. As a transitional use, if any of the facing or adjacent uses are commercial, large institutional, or residential of equal or greater intensity.
- ii. Where facing or adjacent to single-family homes along the same street, the setbacks will be no less than the minimum allowed in the facing or adjacent single-family zoning district and the buildings will employ architectural techniques to reduce the apparent size of the building (see the Residential Compatibility Standards in this chapter).
- iii. There must be off-street parking consistent with City ordinance and also

on-street parking adjacent to the lot to accommodate visitors.

iv. If approved either through the Planned Development (PD) zoning process or Conditional Use Permit (CUP) process.

e. Larger multi-unit buildings with 9+ units or exceeding 20 units per net acre have a place in balanced neighborhoods. These more intensive forms are generally most appropriate close to major streets, mixed-use areas, or commercial / employment areas to provide convenient, walkable access to transit, shopping, restaurants, and other amenities. This plan identifies specific sites for such housing. Properties that are either already intensely developed, or are suitable for more intensive development, have been identified as **High-Intensity Residential (HIR)** Overlay on the Future Land Use Maps, and additional policies apply. See the High-Intensity Residential Overlay section in this chapter.



Development exceeding 20 units per acre may sometimes be acceptable in **NR** areas

not designated with the **HIR** overlay if ALL of the following apply:

- i. The development is not facing or adjacent to single family homes, unless approved as a part of the same development plan.
 - ii. There is adequate parking off-street and on-street adjacent to the development to meet the needs of the development without reducing on-street parking availability in front of other uses.
 - iii. If approved through the Planned Development (PD) zoning process.
 - iv. If consistent with the Residential Compatibility Standards in this chapter.
6. Within most neighborhood areas there should be on-street parking available for use by residents and visitors, typically adjacent to each parcel, to accommodate parking for visitors. Excessive driveway and curb cut widths that eliminate on-street parking space in front of a home should be avoided whenever possible because it reduces the flexibility of the street to accommodate fluctuating parking demand and also because it results in snow storage problems in the winter. A maximum curb cut width of 20 feet is encouraged, even if the driveway widens on the parcel. Use of shared driveways and/or alleys is also encouraged.

Potentially Acceptable Zoning Districts

- Suburban Residential - 4 (SR-4)
- Mixed Residential - 8 (MR-8)
- Urban Residential - 12 (UR-12)
- Planned Development (PD)

BEST PRACTICE DESIGN STRATEGIES

The images and text below describe design strategies for single-family, duplex and multi-unit buildings.

SINGLE-FAMILY & DUPLEX UNITS

A. Building setbacks will vary according to building type and lot size but should generally be consistent within a given block.

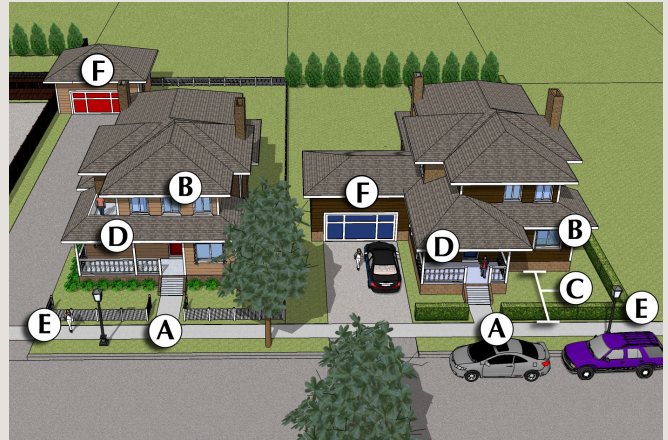
B. Homes should be designed with architectural details that provide visual interest and human scale for the street and the neighborhood. The mix of architectural themes or styles should generally be consistent within a neighborhood or development, but there should be variation in floor plan, facade design, and color choice to avoid monotony. Overly complicated rooflines with multiple pitches and hips and/or excessive gables can negatively impact a street and should be discouraged (see weak and desired design examples on the following page).

C. Utilize low fences, hedges or other landscaping to establish a layer of privacy behind a sidewalk and residence, if a sidewalk is present.

D. Incorporate covered front porch or at least a raised stoop, preferably covered and constructed with materials that relate to the overall design of the home.

E. Decorative fencing and/or landscaping that visually defines the single family lot at the street edge are encouraged.

F. Consider garage location and scale to avoid a “garage-scape” street appearance. Garages should extend no further than the front facade of the residence. If this is not feasible, garages should be turned 90 degrees with windows provided on the side of the garage facing the street.



MULTI-UNIT BUILDINGS

A. The front door should face the street and there should be a clear route to the door from the street or sidewalk.

B. The mix of architectural themes or styles should generally be consistent within a neighborhood or development, but there should be variation in floor plan, facade design, and color choice to avoid monotony. When adjacent to lower density residential buildings, larger buildings should incorporate strategies to minimize the apparent size of the building, including flat roofs instead of pitched roofs, deeper setbacks for upper stories, and/or variation in the depth of the setback along the building facade. Large, undifferentiated building walls and rooflines are strongly discouraged. Desired architectural details include projecting bays/porches and upper-level set-backs and offsets to the primary facade(s).

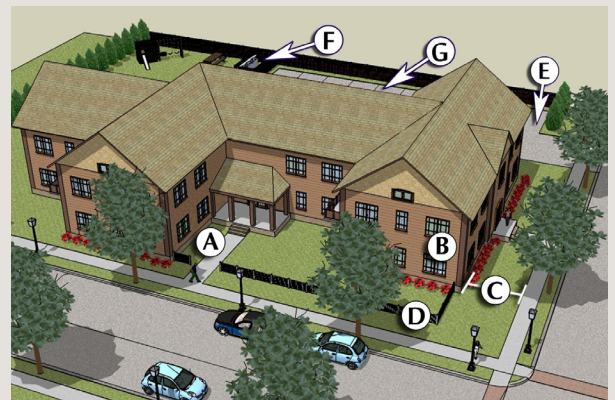
C. Building setbacks will vary according to building type and lot size but should generally not exceed 40 feet.

D. Utilize low fences, hedges or other landscaping to establish a layer of privacy behind the sidewalk.

E. Off-street parking is located in the side and rear yard.

F. Service areas and refuse containers should be located at the rear end of the site and screened from public view. Also ground mounted or wall mounted mechanical equipment shall be screened.

G. Garage doors facing the primary street are discouraged. If unavoidable, recess them from the front facade(s) to minimize their visual impact on the design.



HIR

High-Intensity Residential Overlay

HIR overlay identifies properties or areas in the **Neighborhood Residential (NR)** future land use areas that are suitable for high-intensity residential development. The objective is to provide a mix of housing types to provide for balanced neighborhoods, while mitigating negative impacts to existing or planned low-intensity residential areas. For the purposes of this overlay, low-intensity residential includes single-family, duplex, small multi-unit buildings (up to 8 units) and rowhouse developments (up to 6 units). In general, high-intensity residential development is identified close to major streets, mixed-use areas, or commercial/employment areas to provide convenient, walkable access to transit, shopping, restaurants, and other amenities.

1. Properties identified in the **HIR** overlay can provide both rental and owner-occupied housing, and ideally provide options for people of all ages who wish to live within a neighborhood.
2. High-intensity residential development in the **NR** areas are expected to range 20-40 units per net acres (excluding streets, parks, outlots, etc.).
3. Intensive residential development will require special attention to the design where the use adjoins less intense residential development per the recommended Residential Compatibility Standards outlined on the next page, or as required in the City's zoning ordinance (should the ordinance be amended to include standards). Standards identified in the zoning ordinance shall supersede those outlined on the next page.

Potentially Acceptable Zoning Districts

- Planned Development (PD)

The images at right are examples of weak and desirable design for larger multi-unit structures. See the previous page for design techniques that utilize best planning and design practices.

WEAK DESIGN



DESIRED DESIGN



RESIDENTIAL COMPATIBILITY STANDARDS

A. Purpose. These standards provide a proper transition and compatibility between low-intensity residential development and more intense multi-unit residential, office and mixed use development. For purposes of this section, low-intensity residential development shall mean single-family, duplex, townhomes (6 or less units), and small multi-unit buildings (8 or less units).

B. Applicability. These residential compatibility standards shall apply to all new multi-unit residential, office and/or mixed use development of three-stories or larger and/or any development requiring a Planned Development (PD) zoning approval located on land abutting or across a street or alley from low-intensity residential. *These standards do not apply to development governed by an existing General Development Plan (GDP), but they may be considered if a GDP is amended, especially as they pertain to aspects of the development that are proposed for revision in the amendment.*

C. Compatibility Standards. All development subject to this section shall comply with the following standards:

1. **Use Intensity.** In developments with multiple buildings with varying intensities, the development shall locate buildings with the least intense character (e.g., lower heights, fewer units) nearest to the abutting low-intensity residential development.
2. **Building Height.**
 - i. To ensure that new buildings are compatible in scale with surrounding properties, building height is limited to four (4) stories above ground level.
 - ii. The height of the proposed structure(s) shall not exceed thirty-five (35) feet in height in the following locations:
 1. Within fifty (50) feet of a single-family or duplex lot.
 2. Within twenty-five (25) feet of any other low-intensity residential lot (i.e., structures with 3+ units).
3. **Bulk and Mass.** Primary facades abutting or across a street or alley from low-intensity residential development shall be in scale with that housing by employing the following strategies:
 - i. Varying the building plane setback a minimum of two (2) feet at an interval equal or less than the average lot width of the applicable low-intensity residential uses. *For example, if a block of single-family lots is across the street from the development with an average lot width of 50 feet, the applicable facade shall vary its building plane, at a minimum, every 50 feet.*
 - ii. Providing a gable, dormer, or other change in roof plane at an interval equal or less than the average lot width of the applicable low-intensity residential uses. *For example, if a block of single-family lots is across the street from the development with an average lot width of 50 feet, the applicable roofline shall vary, at a minimum, every 50 feet (measured at the roof eave).*
4. **Roof Pitch.** The roof pitch of new residential buildings shall range between 6:12 and 12:12. The roof pitch of porches shall not exceed that of the residential building to which it is attached.
5. **Architectural Features.** At least two (2) of the following categories of architectural features shall be incorporated into street-facing facades:
 - i. Porches or porticos
 - ii. Balconies
 - iii. Dormers
 - iv. Gables
 - v. Bay Windows
 - vi. Door and Window Ornamentation which may include surrounds, pediments, lintels and sills, hoods, and/or shutters.
6. **Windows.** Windows shall make up twenty (20) percent of the total surface area of street-facing facades.
7. **Entrances.** Street-facing facades providing direct access to first story dwelling units through individual entrances are encouraged. Preference is between twenty-five (25) percent and fifty (50) percent (or greater) of units having direct access.
8. **Garages.** Attached garages shall not face or open towards the street. If this is not attainable, garages shall be sufficiently screened and face the street with the highest intensity of adjacent uses (if on a corner lot).
9. **Parking.** Parking areas that are visible from the street and located in the building front lot setback shall provide buffering at a minimum height of thirty-six (36) inches above the parking surface. Buffering can consist of landscaping, berms, fences/walls, or a combination of these.
10. **Refuse Areas.** Dumpsters shall be placed behind the building with opaque or semi-opaque screening (at a minimum, a chain link with fabric screening). If the refuse area cannot be placed behind the building, a wood fence or wall, at least six (6) feet in height, shall be required. Additional landscaping around trash enclosures is encouraged.

NMU

Neighborhood Mixed Use

NMU category includes relatively small existing and planned activity centers with retail, restaurant, service, institutional and civic uses primarily serving nearby residents. Residential is also a component of the **NMU** district - both in mixed use developments and as stand-alone multi-unit residential developments.

Development and design within **NMU** areas should be compact and walkable. **NMU** areas should be well-connected and integrated into neighborhoods.

1. As part of the zoning approval process, the appropriate mix of land uses, densities, and intensities will be determined with consideration of market conditions and compatibility with adjacent neighborhoods. Typically residential densities in **NMU** areas will be 25-50 units per net acre (excluding streets, parks, outlots, etc.).
2. While both residential and non-residential uses are accommodated within this mixed-use district, not every building in a mixed-use district needs to include both residential and non-residential uses. However, special attention should be paid to maintaining com-

mercial street frontages along mixed-use streets without creating residential “gaps” along streets that otherwise have commercial tenants at ground level. Nonresidential development within **NMU** areas should be service and retail to support surrounding residential uses, as well as attract a wide customer base.

3. An individual building should not include more than 25,000 square feet of commercial space, except for buildings containing grocery stores and/or community facilities (such as libraries). When larger commercial uses are present, the building should still be designed with extra care to ensure compatibility with the surrounding neighborhood. Commercial spaces should be constructed in a range of sizes to add variety and encourage a mix of different commercial uses.
4. New buildings in **NMU** areas are expected to be one to four stories in height with a preference towards multi-story buildings.
5. Mixed use and other intensive development will require special attention to the design where the use adjoins less intense residential development per the recommended Residential Compatibility Standards in this chapter (or as adopted in the City’s zoning ordinance).
6. Integration of affordable housing into mixed-use areas is encouraged, especially along major transit corridors. Multi-unit residential within the mixed-use category should contain a mixture of unit sizes and a mix of income-qualified and market rate units.

7. One-story gas stations with an accompanying convenience store may be considered in newly developing **NMU** areas if the proposed development is designed in a manner that does not impede or substantially detract from the existing or planned development in the surrounding area.
8. Drive-thru establishments may be allowed in **NMU** areas if designed to mitigate the typical auto-centric design, including placing the building close to the street with a public entrance from the public sidewalk and placing the majority of the parking and drive-thru lane facility along the back or side of the building.
9. Buildings in **NMU** areas should be oriented towards streets with minimal setback from the public sidewalks.
10. Private off-street parking should be located primarily behind buildings, underground, or shielded from public streets by liner buildings or substantially landscaped.
11. Any such development should integrate site design elements that facilitate pedestrian and bicyclist access to the retail portion.

Potentially Acceptable Zoning Districts

- Urban Residential - 12 (UR-12)
- Urban Commercial (UC),
- Neighborhood Commercial (NC)
- Planned Development (PD)

DID YOU KNOW?

The mixed-use land use categories allow two or more different land uses on a single tract of land, within a single building, or within separate buildings in close proximity to each other.

BEST PRACTICE DESIGN STRATEGIES

The images and text below describe design strategies for **Neighborhood Mixed Use** buildings and sites.



1.5- to 2-story neighborhood commercial with side-yard parking.



Two-story facade with gabled roof to blend with residential development.



First floor retail with upper story office or residential with shallow setbacks.



Mixed use developments provide two or more uses either on a single tract of land (i.e., horizontal mixed use - see McHenry Development below), or within a single building (i.e., vertical mixed use). Typically the first floor is retail, but office or a public use may also occupy the first floor.



Townhomes with zero front- and side-yard setbacks.



As shown in the images on the left, mixed use developments can come in variety of sizes and styles that can complement the surrounding commercial and residential character.



Home-to-office conversions are a good transition between single family residential and non-residential uses.



Elements to consider: building height, roofline (pitched vs. flat), building materials, building setback from the street, parking location, etc.



McHenry Development ("Horizontal" Mixed Use)

CMU

Community Mixed Use

CMU category includes existing and planned areas supporting an intensive mix of residential, commercial and civic uses serving residents and visitors from the surrounding area and the community as a whole. **CMU** areas are generally located at major intersections and along relatively high-capacity transit corridors, providing more residential units and commercial space compared with development in **NMU** areas. **CMU** areas can generally accommodate significant development with a variety of housing options designed to support nearby employment areas by providing residential units close enough to make walking and biking a convenient method of commuting.

Many of the City's auto-oriented strip commercial centers are recommended for **CMU** redevelopment due to their accessible locations along major transportation corridors and the opportunities to significantly increase integrated housing and commercial development.

1. The zoning approval process should determine the appropriate mix of land uses, unit density, and building size and configuration. Typically residential

densities in **CMU** areas will be 30-60 units per net acre (excluding streets, parks, outlots, etc.).

2. While both residential and non-residential uses are accommodated within this mixed-use district, not every building in a mixed-use district needs to include both residential and non-residential uses. However, special attention should be paid to maintaining commercial street frontages along mixed-use streets without creating residential "gaps" along streets that otherwise have commercial tenants at ground level. Nonresidential development within **CMU** areas should be service and retail to support surrounding residential uses, as well as attract a wide customer base.
3. Gas stations and drive-thru establishments should not typically be permitted in **CMU** areas.
4. Subject to adopted detailed neighborhood plans, **CMU** areas are intended to include buildings two- to six-stories in height.
5. Development and design within **CMU** areas should prioritize pedestrian access, including generous sidewalks, welcoming street entrances, and con-

siderations for the location of transit stops.

6. Intense development in **CMU** areas may require structured parking. Any surface parking should be screened from the street, preferably by buildings.
7. Mixed use and other intensive development will require special attention to the design where the use adjoins less intense residential development per the recommended Residential Compatibility Standards in this chapter (or as adopted in the City's zoning ordinance).
8. Integration of affordable housing into mixed-use areas is encouraged, especially along major transit corridors. Multi-unit residential within the mixed-use category should contain a mixture of unit sizes, and a mixture of income-qualified and market rate units.

Potentially Acceptable Zoning Districts

- Urban Residential - 12 (UR-12)
- Urban Commercial (UC),
- Neighborhood Commercial (NC)
- Planned Development (PD)

DID YOU KNOW?

The mixed-use land use categories allow two or more different land uses on a single tract of land, within a single building, or within separate buildings in close proximity to each other.

BEST PRACTICE DESIGN STRATEGIES

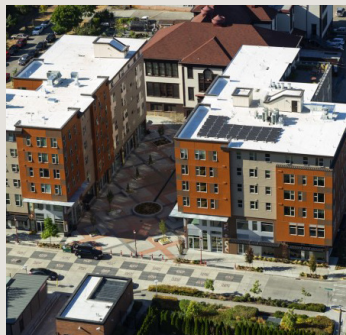
The images and text below describe design strategies for **Community Mixed Use** buildings and sites.



This two-story building represents the lowest intensity mixed use development in CMU areas. This development type provides a good transition between lower-density neighborhood residential districts and high-density developments (see example to the right).



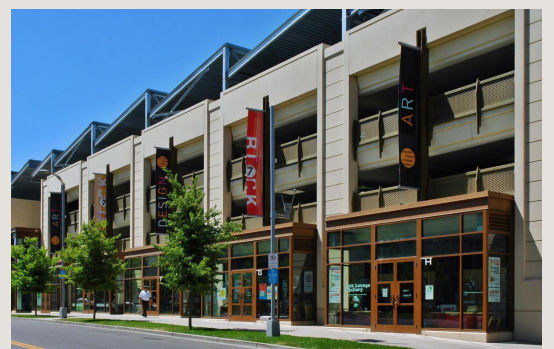
This mixed use building represents the greatest intensity and massing plausible in Sun Prairie with its likely placement near USH 151. A neighborhood plan shall provide design features and uses that buffer intensive development such as this from lower-density neighborhoods.



Outdoor social gathering spaces should be an integral part of CMU areas to support residential populations in the near vicinity, as well as provide for gathering of residents from the surrounding areas as a community destination and activity center.



Good Design Elements: Vertically-proportioned facade, articulation between street-level commercial and residential units above, significant glass on street-level, varying roofline, high quality building materials, and ample landscaping.



Design for a parking structure along a public street should provide visual interest compatible with adjacent developments and is encouraged to provide occupiable spaces along the street level.

DMU

Downtown Mixed Use

DMU category represents the entirety of downtown Sun Prairie and accommodates a wide variety of employment, service, retail, government, entertainment and residential uses in multi-story buildings. The general intent of the **DMU** area is to preserve the architectural character of the historic commercial district, while providing significant density and intensity of uses befitting the growing City of Sun Prairie. The core blocks fronting on Main Street should continue to maintain buildings with their front facades built to the edge of the public sidewalk.

1. As part of the zoning approval process, the appropriate mix of land uses, densities, and intensities will be determined with consideration of market conditions and compatibility with adjacent neighborhoods. Typically residential densities in **DMU** areas will be 50-80 units per net acre (excluding streets, parks, outlots, etc.).
2. **DMU** is best suited for densely populated residential developments, mixed use developments with first-floor retail, service and office users, and destination businesses (e.g., restaurants, bars and entertainment venues).
3. Building heights may range from two- to six-stories, except in the following conditions:
 - A. Along Main Street, a detailed cornice shall be established above the third story with remaining floors set back from the primary façade by a minimum of two (2) feet. The intent of this architectural feature is to maintain the “small-town” and historical scale of

Main Street.

B. New developments adjacent to lower-intensity uses along South Street, North Street, Vine Street and Dewey Street shall be restricted to four story maximum height. Other architectural design considerations (e.g., greater rear setbacks, upper story rear setbacks, etc.) may be required to mitigate negative impacts to the neighboring residential areas.

4. Restrict new residential uses on the street level along Main Street and discourage on adjoining commercially zoned streets. Office users may locate on the street level; however, upper-level office use is preferred on Main Street.
5. Require the architecture of any new development in Downtown Sun Prairie to be compatible in terms of architectural character and materials within the corresponding block face.
6. Encourage site planning that is “pedestrian-friendly” and provides both sidewalks and interior pedestrian circulation pathways.
7. Relocate existing industries and heavier commercial uses in the Downtown area to more appropriate sites in a business park setting.
8. Protect the character of “transition” blocks within the **DMU** that are at least 50% residential, through the following strategies:
 - A. Allow use to change from residential to commercial, but retain the character and scale of the existing structures and yards.
 - B. Limit commercial uses to the uses permitted in the Urban Commercial (UC) Zoning

District.

C. Require parking to be located in the rear or side yards, preserving lawn in the front yards.

9. If a Downtown Plan is adopted after the 2019 Comprehensive Plan, that plan shall supersede if in conflict with this Plan.

Potentially Acceptable Zoning Districts

- Urban Residential - 12 (UR-12)
- Urban Commercial (UC),
- Neighborhood Commercial (NC)
- Planned Development (PD)

BEST PRACTICE DESIGN STRATEGIES

The images and text below describe design strategies for **Downtown Mixed Use** buildings and sites. If an adopted Downtown Plan contradicts any of these strategies, the Downtown Plan shall supersede these strategies.

Guidelines for the Rehab of Historic Storefronts:

- Avoid altering, concealing, or removing historic details when renovating upper façades.
- If the building has an intact and original storefront, preserve the storefront's character.
- Original window openings should not be concealed and the size and proportion of the original windows and doors should not be altered.
- Avoid the use of materials that were unavailable when the building was constructed, including vinyl and aluminum siding, mirrored or tinted glass, artificial stone, etc.
- Canopies that are not part of the original building design should be avoided.
- Choose paint colors based on the building's historic appearance.
- Paint should only be applied to trim features of masonry and stone buildings. Masonry and stone should be cleaned rather than painted.

Guidelines for New Storefronts:

- The architectural character of buildings on Main Street should reflect traditional architectural themes.
- Provide an architectural separation (e.g., cornice) between the storefront and upper stories.
- Differentiate the primary retail entrance from the secondary entrance to upper floors.
- The storefront generally should be as transparent as possible.
- The façade design of new buildings should complement adjoining buildings in proportion, material selection and color.
- Fabric awnings are desired. Plastic /metal canopies should be avoided.
- Simple color schemes with up to three colors are appropriate. Avoid bright colors or highly contrasting colors.
- Avoid large projecting signs. The scale of signage should be proportional to buildings and be consistent with the pedestrian environment.
- Parking shall not be in the front yard setback, and rear lot parking is preferred over side yard parking.



Tall buildings should step back the upper floors (above the third floor) to reduce the impact on the surrounding properties, and to maintain the historical scale of Main Street.



First floor retail with upper story office or residences is preferred with shallow setbacks to promote walkability.



Two stories (or the appearance of two stories) should be the minimum building height in the DMU area. Development should hold street corners by extending the building facade (and roofline) higher at least one bay deep from the corner.



Design for a parking structure along a public street should provide visual interest compatible with adjacent developments and is encouraged to provide occupiable spaces along the street level.

GC**GENERAL
COMMERCIAL**

GC areas provide the city’s population with a wide range of retail goods and services, including professional offices and daycare facilities. Commercial areas includes automobile-oriented uses and “heavy” commercial uses with the appearance or operational characteristics not generally compatible with residential or small-scale commercial activities.

The type and size of use will be determined by location and business characteristics (e.g. size, hours of operation, traffic impacts, etc.) For example, areas near major transportation routes or near highway intersections are generally better suited for larger retail uses (e.g. exceeding 20,000 square feet in size).

Those areas located along local streets or adjacent to residential neighborhoods are better suited for smaller commercial uses that serve neighborhood needs. Such uses typically require smaller building footprints and parking lots and are less likely to have intensive truck and delivery needs.

GC areas are not generally recommended for residential uses, though such uses may be considered as part of a conditional use under relevant zoning districts.

1. Smaller **GC** areas should provide an attractive interface and convenient pedestrian connections with adjacent residential areas and should be designed to encourage non-auto accessibility.

2. While **GC** areas tend to be auto-oriented, changes to **GC** development that improve walking, biking, and transit access are encouraged.
3. Outdoor storage of raw materials is discouraged particularly if materials are not screened by a solid wall fence or landscaping.
4. Outdoor areas for dining are encouraged while outdoor display of retail merchandise should be minimized.
5. Depending on specific uses, the districts may require significant buffering from adjacent land uses.
6. There is no limit on the size of establishments that may be constructed within a **GC** area, but all uses should be compatible with the density and scale of the surrounding development.

**Potentially Acceptable
Zoning Districts**

- Neighborhood Commercial (NC)
- Suburban Commercial (SC)
- Urban Commercial (UC)
- Suburban Office (SO)

E**EMPLOYMENT**

E areas include predominantly corporate and business offices, research facilities, laboratories, medical clinics/hospitals, and other similar uses. They generally do not include retail and consumer service uses for the wider community, but may include limited retail and service establishments that primarily serve employees and users of the area. Employment areas are not generally recommended for residential uses, though such uses may be considered as part of a conditional use under relevant zoning districts. Although generally used to identify relatively large, multi-establishment employment areas (e.g., Sun Prairie Business Park) the designation may also be applied to an individual property (e.g., clinic).

1. While there are no fixed limits on size of an establishment or development intensity within **E** areas, all uses should be compatible with the density and scale of surrounding development. The intensity of development may vary significantly depending on the location and surrounding context.

**Potentially Acceptable
Zoning Districts**

- Urban Commercial (UC)
- Suburban Office (SO)
- Suburban Industrial (SI)

BEST PRACTICE DESIGN STRATEGIES

The images and text below describe design strategies for **General Commercial and Employment** buildings and sites.



The images above illustrate techniques used to vary the facade heights along a long facade.



This example shows an office building with a primary facade using brick with stone as an accent material, meeting desired natural color palette.



Desired design for a retail building. The color palette includes natural and earth tones. Building uses brick (primary) and EIFS (secondary), and architectural details that breakdown the facade to human scale. There are varying building planes and heights.



Left, retail building with primary facade using fiber cement siding with stone as an accent material. Right, retail building with primary facade using stone and brick with EIFS as an accent material. Both meeting a desired natural color palette.



An example of a commercial building using earth tones as the primary color with brighter accent color for awnings and roofing.



The example above shows a raised parapet wall and cornice that extends back to give the perception of three-dimensional facade (desirable). The example on the right shows a “fake” parapet wall and cornice that is not three-dimensional (as it lacks depth).



An example of a high quality office building using variations in building color, projections and windows to break up long walls.

I INDUSTRIAL

Industrial (I) areas accommodate manufacturing, wholesale, storage, distribution, transportation, and repair/maintenance uses. The designation may also be used for landfills and gravel or mineral extraction activities. Industrial areas can include “nuisance” uses that should not be located in proximity to residential, mixed-use, or some other types of non-residential uses due to noise, odor, appearance, traffic, or other impacts. The Industrial designation is not intended for retail or office uses not related to an industrial use, except for limited retail goods and services provided primarily to employees and us-

ers of businesses within the area. Compared to the E designation, I areas generally have a relatively smaller workforce (for a given area), an emphasis on truck or rail traffic, and other characteristics such as outdoor work areas and outdoor equipment and materials storage.).

compatible and parking/storage areas should be screened from public streets.

- Buildings and site improvements may be more simple and vehicle-oriented than in other land use categories.

- Areas may provide a variety of flexible sites for small, local, or startup businesses and sites for large regional or national businesses.
- Architectural, site design, and landscaping features within I areas may be less extensive than in E areas, though properties should be well-buffered and screened from adjacent land uses that may not be

Potentially Acceptable Zoning Districts

- Suburban Industrial (SI)
- Heavy Industrial (HI)

BEST PRACTICE DESIGN STRATEGIES

The images and text below describe design strategies for **Industrial** buildings and sites.

WEAK DESIGN



DESIRED DESIGN



Left, a metal-faced (or concrete panel) building devoid of any architectural merit or character. Right, an industrial building using stone as a base material and metal siding making up to 75% of the remaining facade.



Using generous amounts of landscaping to screen large industrial buildings from street view.



Variations in facade depth and heights for industrial buildings to break up long monotonous walls.



Monument signs are encouraged in all settings instead of pole signs.

IU Institutional & Utilities

IU areas include cemeteries, schools, community centers, government facilities, railroads, utilities and other parcels that are owned by a public, quasi-public, utility, or religious entity. Park and recreational uses are sometimes a primary or secondary use on these sites.

1. Schools and places of assembly and worship should be located to provide convenient access to such facilities.
2. Larger **IU** uses should be located on or near an arterial or collector street, and be designed so that high volumes of traffic will not be drawn through local neighborhood streets.
3. Streets, walkways, and multi-use paths and trails should provide strong pedestrian and bicycle linkages adjacent to and within larger **IU** areas.
4. If a parcel planned for Institutional use is vacated by that use and another use is proposed, the City may approve an alternative use without amending this plan if the proposed use is similar to and compatible with adjacent uses.

Potentially Acceptable Zoning Districts

Conditional use in most of the City's zoning districts.

POS Parks & Open Space

POS category includes public parks, conservation areas, recreation areas, private recreation uses (e.g., golf courses), cemeteries, stormwater management facilities, greenways, major public trails, and other natural features and lands with a park-like character that are recommended for preservation. As the Future Land Use Map is general in nature, smaller parks (generally less than an acre) may be shown as an adjoining land use. Parks and open space uses are allowed uses in all other land use categories, regardless of whether or not the area is mapped as Parks and Open Space. Note that areas mapped as POS in newly developing parts of the city are preliminary and may be refined as plats are submitted.

1. Parks often serve as important community gathering places, and should be designed to have frontages on public streets that make them both visible and accessible by neighborhood and City residents.
2. Greenways and stormwater conveyances provide opportunities to link otherwise separate open spaces with both habitat corridors and bicycle and pedestrian connections when multiple uses are compatible.

Potentially Acceptable Zoning Districts

City's natural resource protection zoning standards apply to most of these areas.

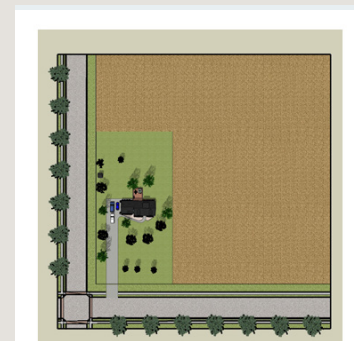
UR Urban Reserve

UR category are lands suitable for eventual urban development within the City of Sun Prairie (likely beyond 2040), but development is restricted due to a lack of utility infrastructure (e.g., Far East Side Interceptor Sewer – See Map 4-1 in Appendix D).

1. Urban development should only proceed after a detailed neighborhood plan has been prepared and adopted for the area.

BEST PRACTICE DESIGN STRATEGIES

Placing the property at the corner of the parcel maximizes productive agricultural land.



Desirable Layout #1



Desirable Layout #2

RL

Rural Lands

RL are recommended for continued agricultural and open space uses. Urban development is not anticipated before 2040. Even if urban development eventually reaches these areas, some of the land in the **RL** area may be appropriate for consideration as permanent agricultural-related uses.

1. Recommended land uses in the rural area land use district are long-term agriculture and related agri-business uses and existing non-farm residential uses served with private, on-site septic systems.

RRP

NATURAL RESOURCE

PROTECTION OVERLAY

NRP overlay classification identifies sensitive lands that may be subject to development restrictions enforced by City, County, State, or Federal agencies. Mapped NRP areas include lands that meet one or more of the following conditions: water bodies and wetlands mapped as part of the WDNR Wetland Inventory, 100-Year Floodplains based on FEMA maps and areas with slopes averaging 12% or more based on USDA-NRCS Soils data.

Areas shown as NRP on the Future Land Use Map do not constitute the limits of all wetlands, floodplains, or steep slopes that may be present within the City's planning area. Mapped NRP areas are derived from third party sources and are generally considered sufficient for the intent of this planning document. They are not a substitute for field or site level delineations that may be required by local, county, state, or federal agencies prior to development approval. The NRP areas illustrated on the Future Land Use Map are not a substitute for official Shoreland-Wetland and Floodplain zoning maps.

The primary intent of these areas is to retain sensitive natural areas in either public or private ownership for the benefit of maintaining fish and wildlife habitat, prevent-

ing and controlling water pollution, preventing erosion and sedimentation, preventing property damage caused by flooding, preserving areas of natural beauty, and providing areas for outdoor recreation. A majority of the NRP represents areas that are vital to the region's ecosystem and are key ingredients of the character and image in Sun Prairie. Thus, development in areas designated NRP shall be limited based on underlying local, county, state or federal environmental regulations.

This classification is intended to function as an overlay district, such that the underlying future land use classification (e.g., General Commercial) remains in place, but the overlay classification indicates the possibility of additional restrictions on development.

Landowners and developers are advised that land within NRP areas may be restricted from building development, site grading, or vegetation clearing under local, county, state, or federal regulations. Where building development is permissible additional building setbacks and buffer yards beyond the minimum requirements are encouraged. Recreational uses, agricultural and silviculture operations may be permitted in accordance with local, county, state, and federal laws. Best Management Practices are highly encouraged in these areas.

FUTURE LAND USE MAPS

This section includes Future Land Use Maps by planning jurisdiction, municipal limits (as of January 2019), and by planning area (as developed during the 2019 comprehensive planning process - See Figure 9-2).

The future land use planning area maps provide greater detail at a neighborhood scale, including:

- identification of development limitations (i.e., Natural Protection Resource Overlay);
- description of detailed neighborhood concepts for currently undeveloped lands - per the 2009 Land Use Workshop; and,
- map notes providing current (2019) narrative of ideas, preferences, and applicable plans and zoning requirements.

The location of future land uses on the Planning Area maps are identical to the Planning Jurisdiction and City-wide Future Land Use Maps.

Figure 9-2: Planning Areas

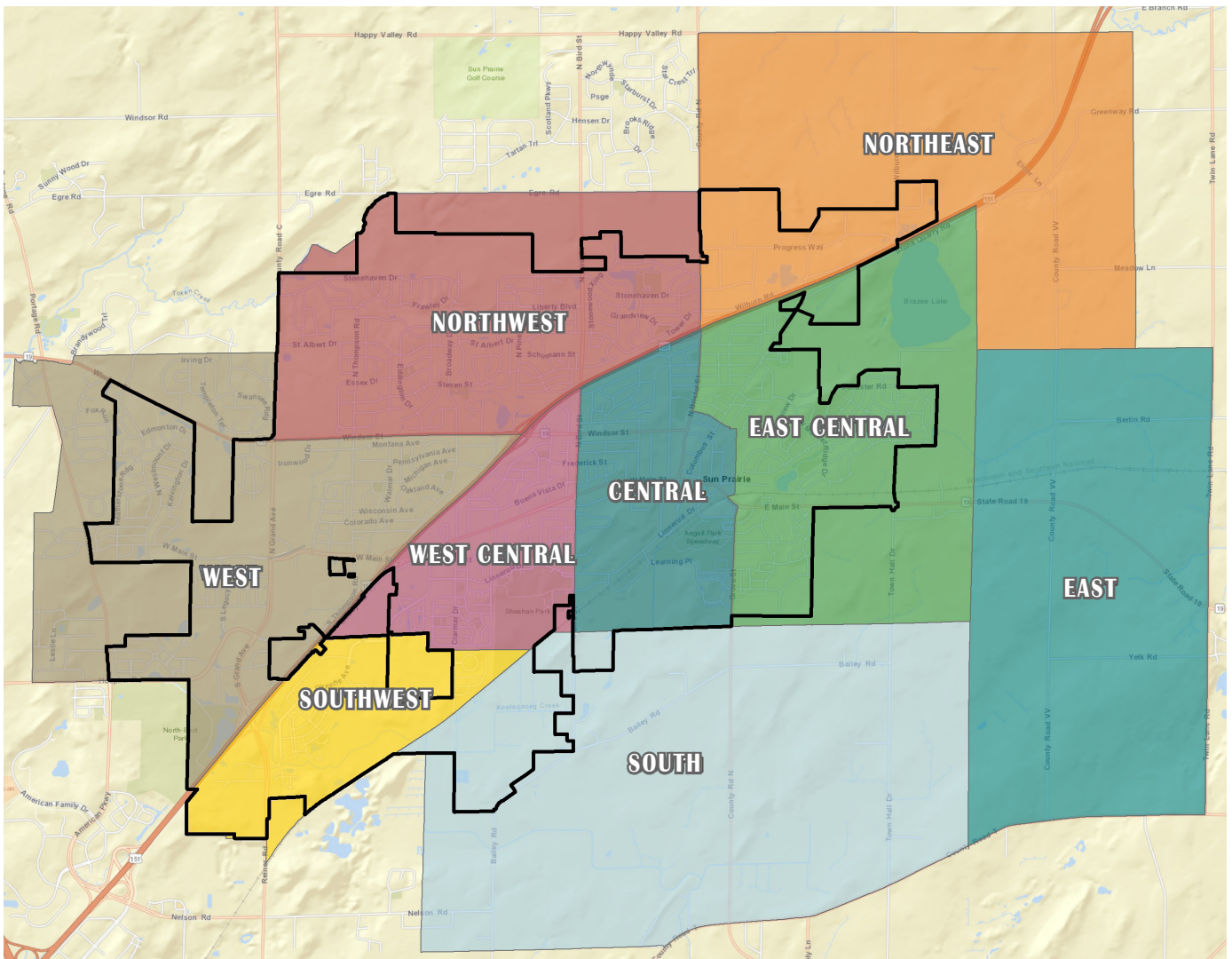
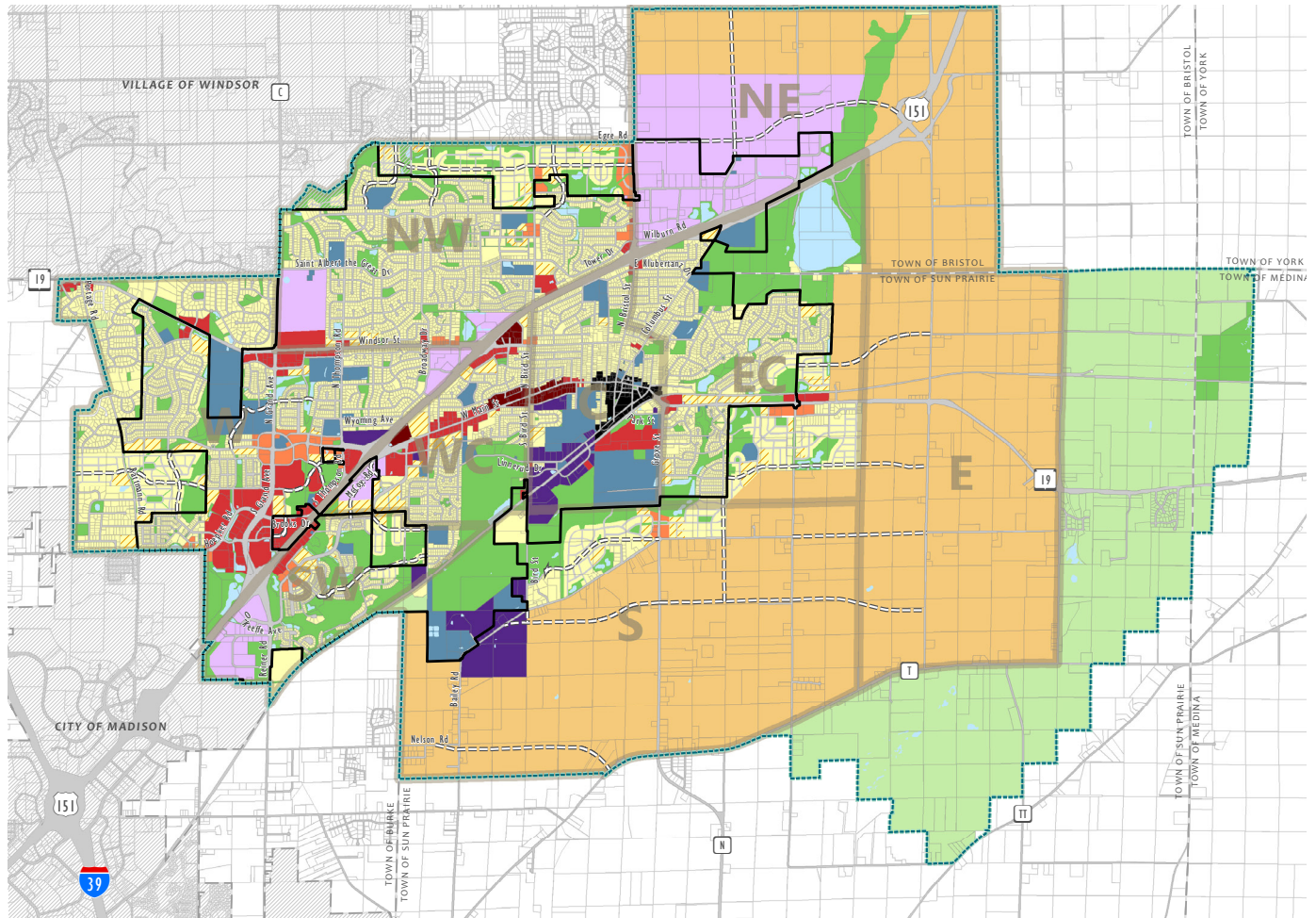







Figure 9-3: Future Land Use Map - Planning Jurisdiction (See Map 9-3 in Appendix D)



-  Planning Areas*
-  City of Sun Prairie
-  City/Village
-  Town
-  Future Arterial/Collector

Future Land Use (2019)











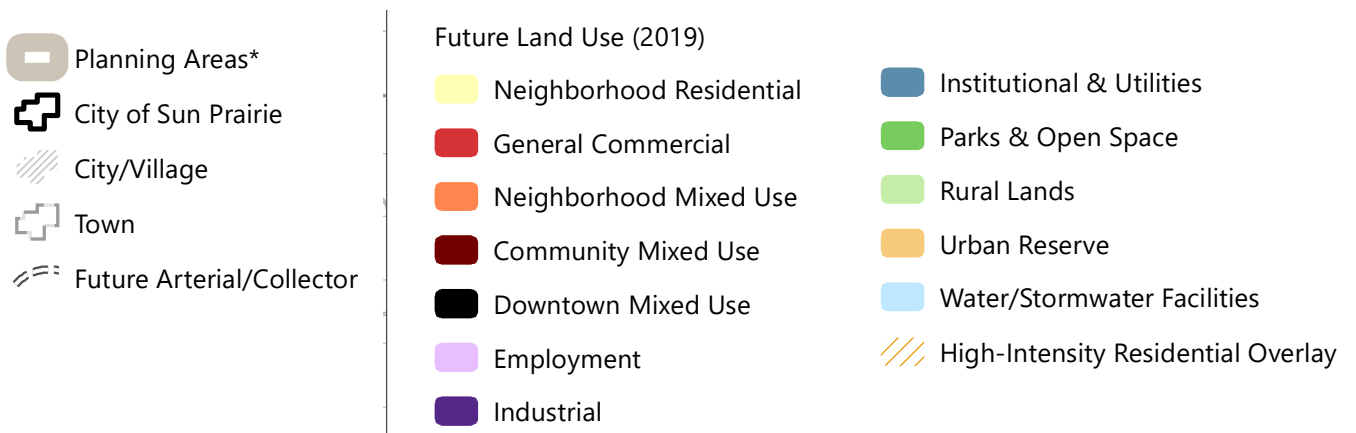
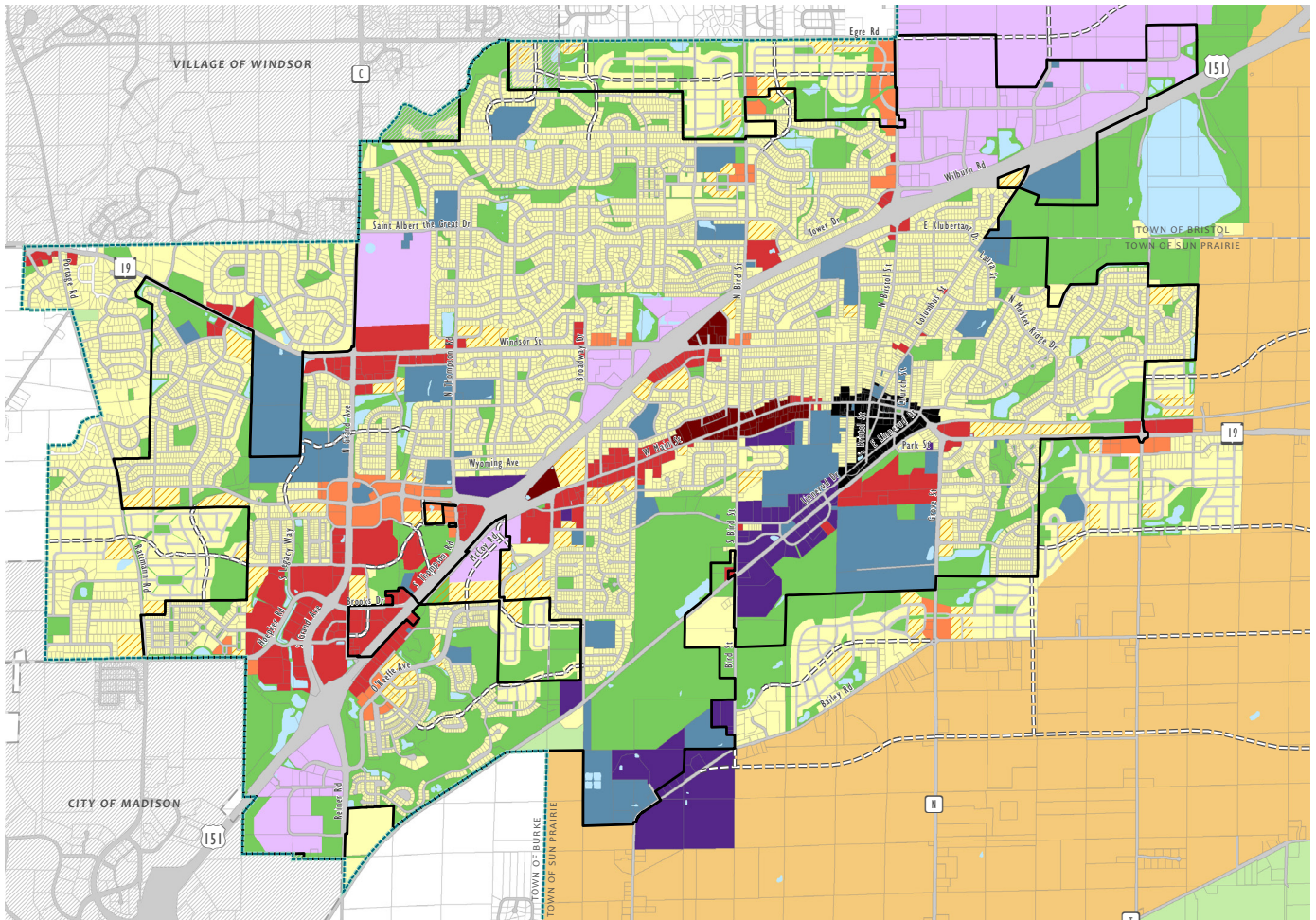
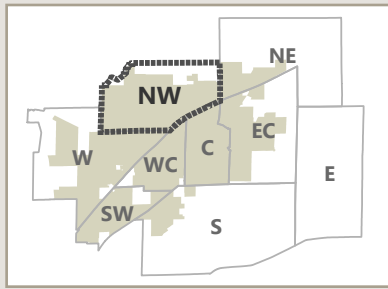
- | | |
|--|--|
|  Neighborhood Residential |  Institutional & Utilities |
|  General Commercial |  Parks & Open Space |
|  Neighborhood Mixed Use |  Rural Lands |
|  Community Mixed Use |  Urban Reserve |
|  Downtown Mixed Use |  Water/Stormwater Facilities |
|  Employment |  High-Intensity Residential Overlay |
|  Industrial | |

Figure 9-4: Future Land Use Map - City Limits (2019) (See Map 9-4 in Appendix D)



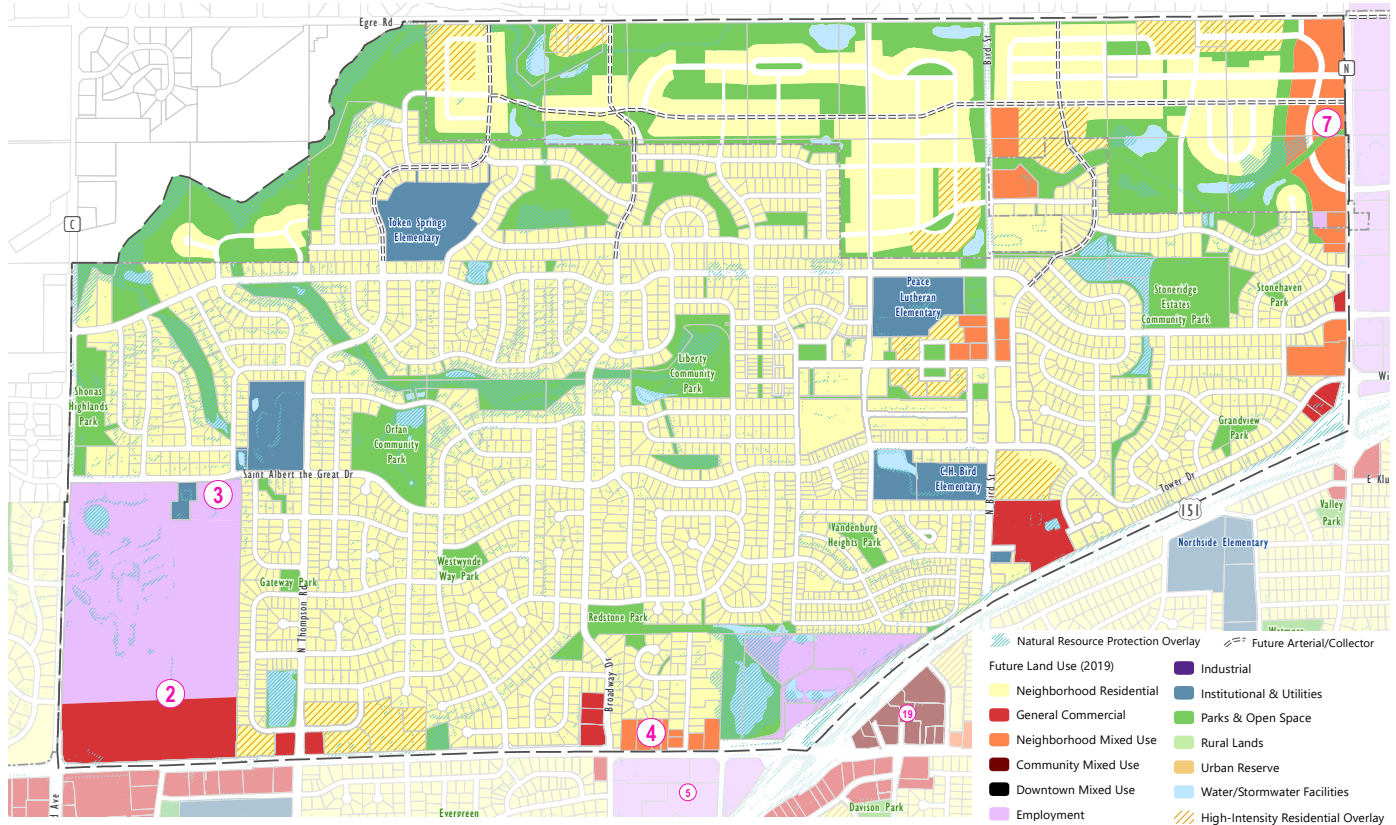
Northwest Planning Area



Planning Area Boundary: Egre Road to the north, CTH N to the east, USH 151 and WIS 19 to the south, and CTH C to the west.

Existing Land Use(s): Mixed residential, commercial, vacant platted lands, and agriculture.

Figure 9-5: Northwest Planning Area (See Map 9-5 in Appendix D)



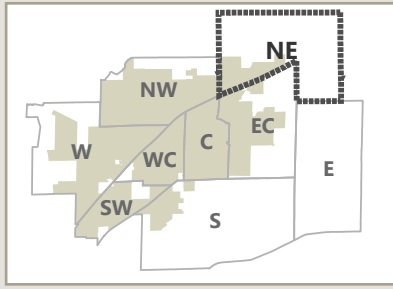
Detailed Concept (2009)

This planning area includes undeveloped lands on the north side of the City, south of Egre Road. Most of this planning area falls within the Token Creek watershed and development within this area must be sensitive to this environmental feature. Any development within this area would need to incorporate a large percentage of open space to manage stormwater appropriately within the watershed. This was a requirement of the Capital Area Regional Planning Commission in order to expand the urban service area for the The Reserve, and it is anticipated that similar measures will be required for any additional expansions. The area east of Bird Street could be developed at a higher-intensity, however existing land uses adjacent to this area are predominantly residential, with the exception of the Sun Prairie Business Park east of CTH N. Future development in this area will need to take into account these neighboring uses. There are two neighborhood mixed use nodes planned in this conceptual plan: 1) along Bird Street; and, 2) along CTH N.

FUTURE LAND USE NOTES

- 2 Development opportunity within the open lands surrounding the QBE Campus for office and commercial. Office (instead of general commercial) is another option along Windsor Street.
- 3 The woodlands in this area should be preserved.
- 4 Existing large single family lots along an arterial street. Potential for transition to commercial uses and removal of direct access from STH 19.
- 7 The 2009 Comprehensive Plan developed a conceptual plan for this area. The 2019 update has reclassified this area as Neighborhood Mixed Use to provide opportunities for housing (including workforce housing) and commercial to support the Sun Prairie Business Park and the adjacent residential neighborhood.

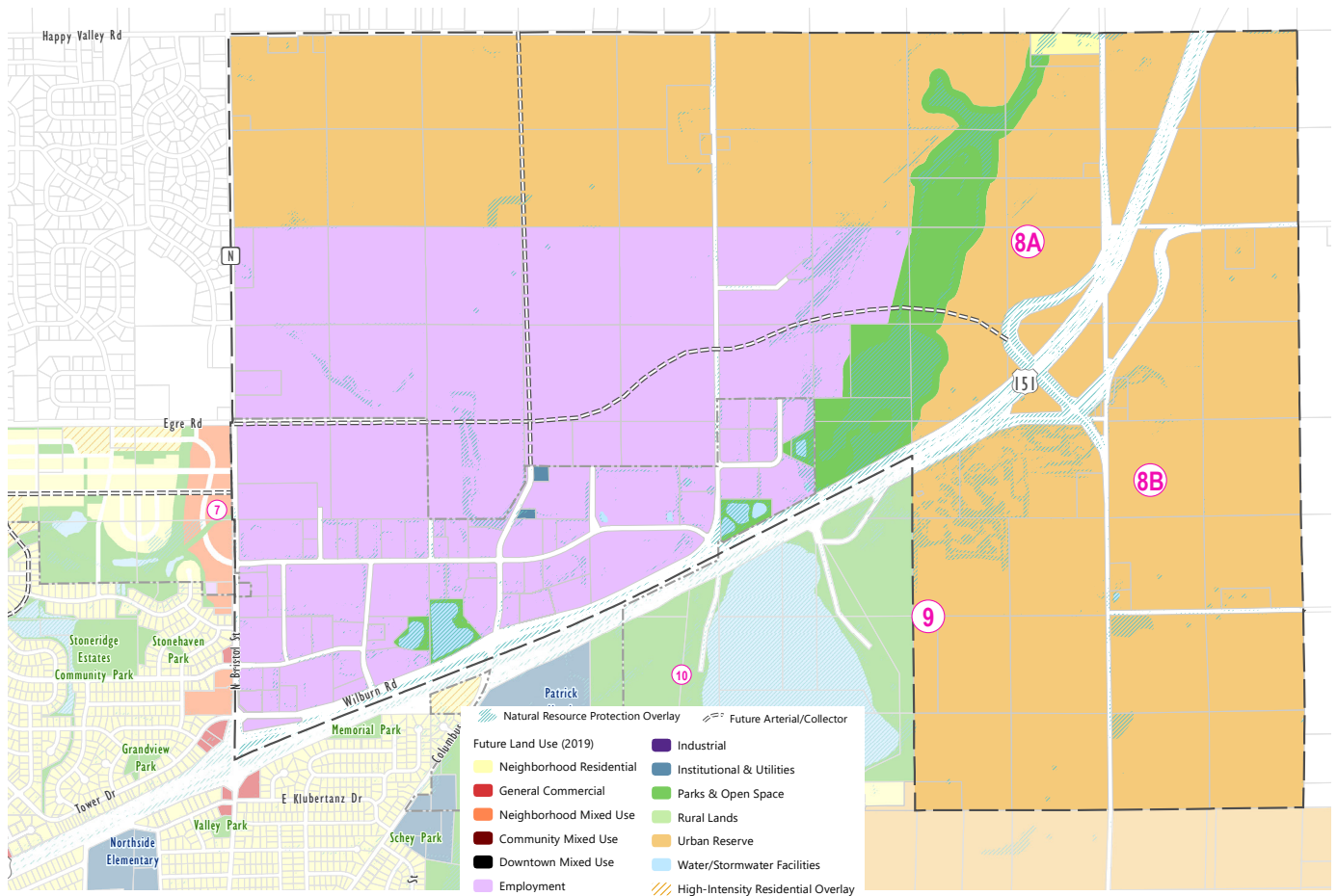
Northeast Planning Area



Planning Area Boundary: Happy Valley Road to the north, USH 151 to the east and south, and Bristol Street to the west.

Existing Land Use(s): Light-Industrial within the City limits (i.e., Sun Prairie Business Park) and Agriculture use outside of the current municipal boundary.

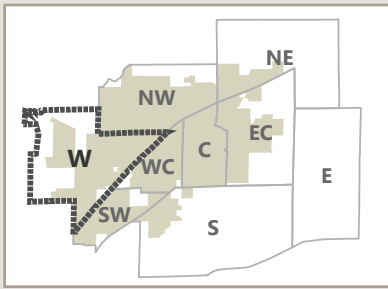
Figure 9-6: Northeast Planning Area (See Map 9-6 in Appendix D)



FUTURE LAND USE NOTES

- 8A 8B Community mixed use requiring a detailed planning effort shall be undertaken in conjunction with planning for an USH interchange at CTH VV.
- 9 This area provides an opportunity for future residential lots with water views, but is not recommended for development before 2040.
- 10 Consider public facilities that allows better access and usability of the Patrick Marsh. Possible location for a Public/Private Event Venue or (Multi-) Cultural Center.

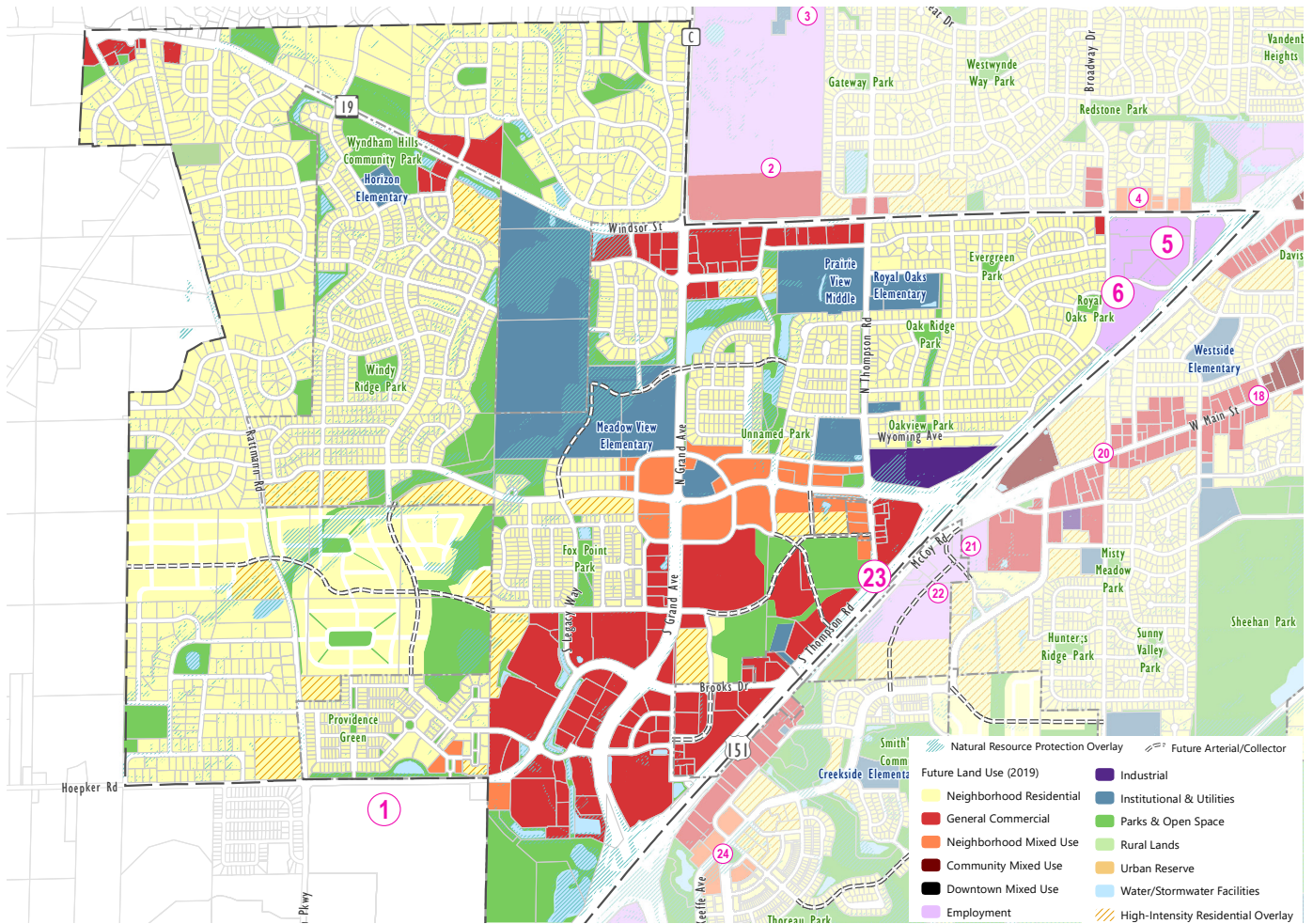
West Planning Area



Planning Area Boundary: Irving Drive and WIS 19 (Windsor Street) to the north, USH 151 to the east, Hoepker Road and USH 151 to the south, and halfway between Rattman Road and Portage Road to the west.

Existing Land Use(s): Mixed residential, commercial, vacant platted lands, and agriculture.

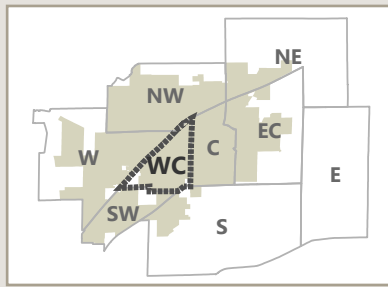
Figure 9-7: West Planning Area (See Map 9-7 in Appendix D)



FUTURE LAND USE NOTES

- ① Potential sports and recreation facility, located in and shared with the City of Madison. Could include a splash pad.
- ⑤ Vacant parcels along Communications Drive are high-priority development sites. The City desires high-quality, multi-storied office buildings in this location.
- ⑥ This site (301 Broadway Dr) is a high-priority redevelopment site. The City will consider proposals for residential in the designated Employment area along Broadway Drive without requiring a plan amendment. If adding residential units, consider the need for additional park and open space in this area.
- ②③ The City will reach out to WisDOT to seek funding support for a USH 151 overpass in this area when development is proposed on either side of the highway (i.e., along S. Thompson Rd and/or McCoy Rd).

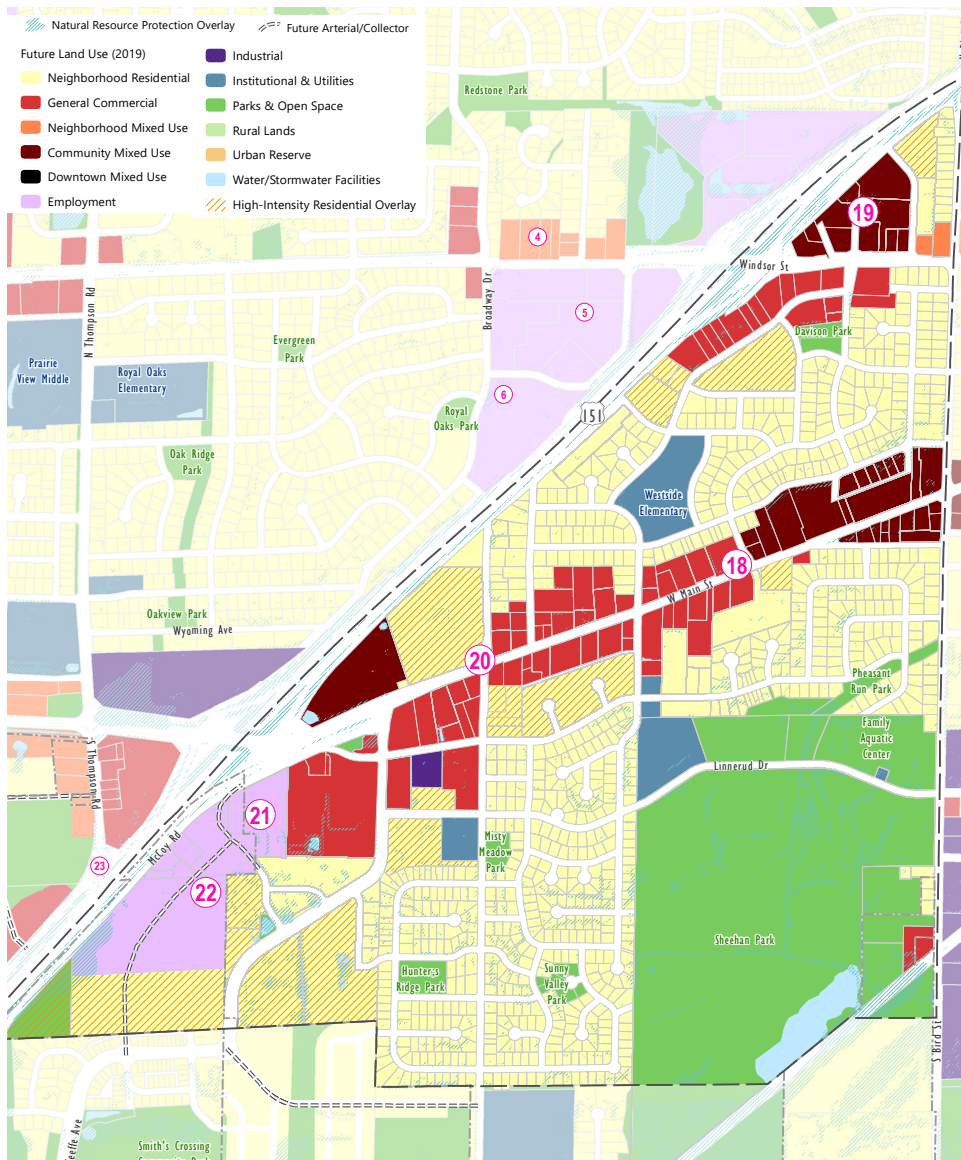
West Central Planning Area



Planning Area Boundary: USH 151 to the west and to the north, Bird Street to the east, and Sapphire Way and O’Keeffe Avenue to the south.

Existing Land Use(s): Mix of residential types, commercial and park and open space.

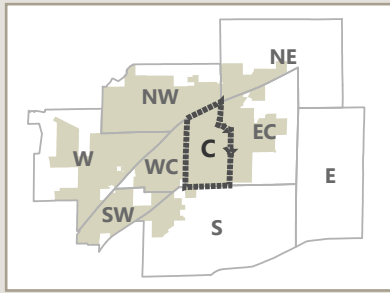
Figure 9-8: West Central Planning Area (See Map 9-8 in Appendix D)



FUTURE LAND USE NOTES

- 18 Properties along Main Street from Clamar Road to Union Street are in a Main Street Overlay District (MSO) that includes site and building design guidelines for future redevelopment and building expansion projects along this corridor.
- 19 This shopping center is currently auto-centric and could be a redevelopment opportunity that provides a pedestrian-friendly mixed use center with housing and greater variety of commercial uses.
- 20 There are two plans that address desired uses, development types and overall character along this section of Main Street - USH 151 to Ruby Ln: West Main Street Corridor Plan (2006); Ruby Ln to Jones St: Central Main Street Corridor Redevelopment Plan (2018).
- 21 This property (2061 McCoy Rd) is a redevelopment opportunity. The City will consider proposals for general commercial without requiring a plan amendment.
- 22 High-quality, multi-storied office buildings is the preferred use and development type, especially adjacent to USH 151. The City will consider proposals in this Employment area for high-intensity residential (if not adjacent to USH 151) without requiring a plan amendment. Should this area redevelop with intense development it will be important to provide an internal road network that links to Prairie Run with an improved controlled intersection at Prairie Run and O’Keeffe.
- 23 See West PA (on the previous page)

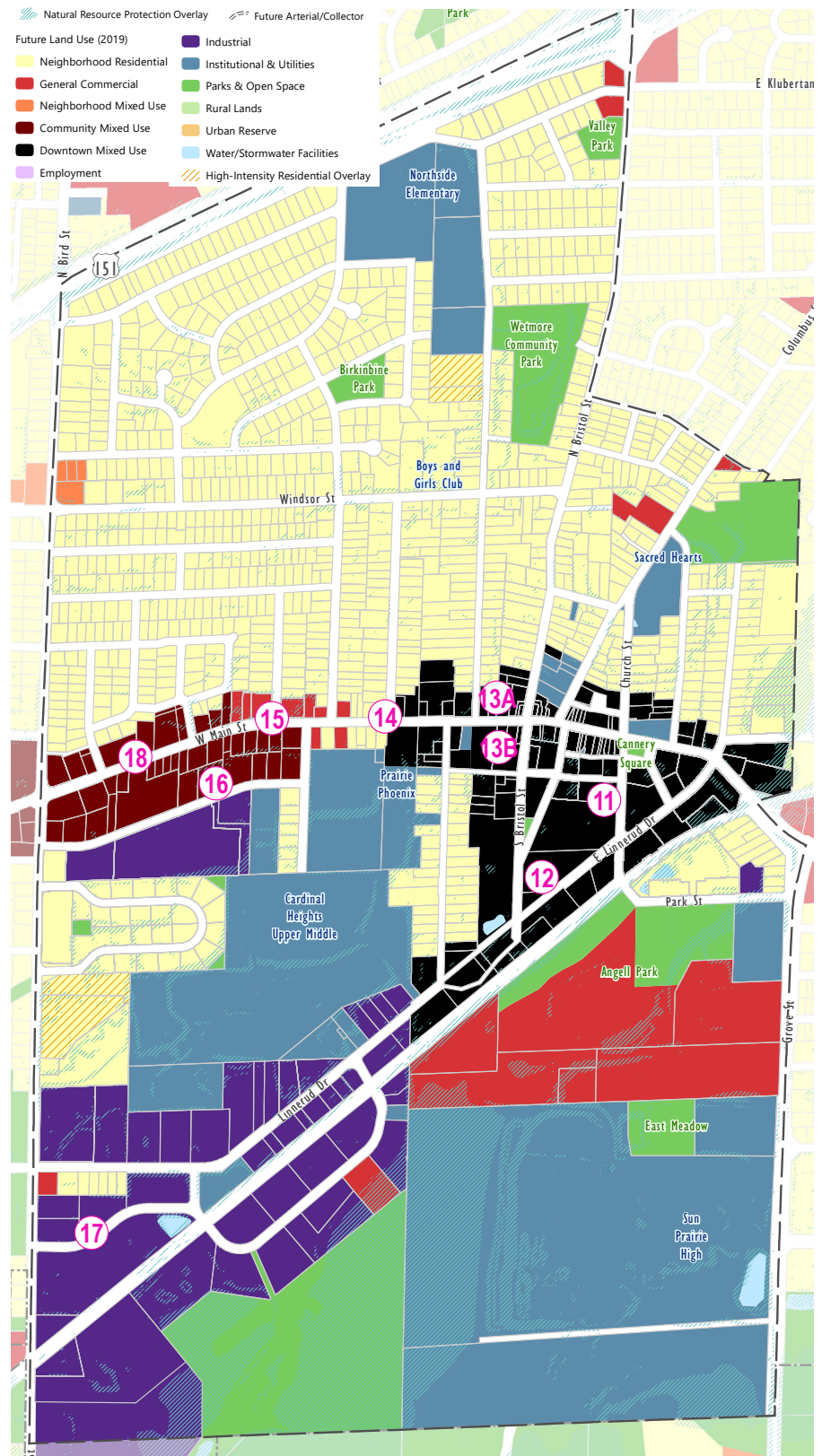
Central Planning Area



Planning Area Boundary: USH 151 to the north, N Bristol and Grove Streets to the east, Sun Prairie High School to the south, and Bird Street to the west. *This sub-planning area includes the Downtown.*

Existing Land Use(s): Single- and two-family residential to the north, mix of uses in the downtown, and industrial along Linnerud Drive.

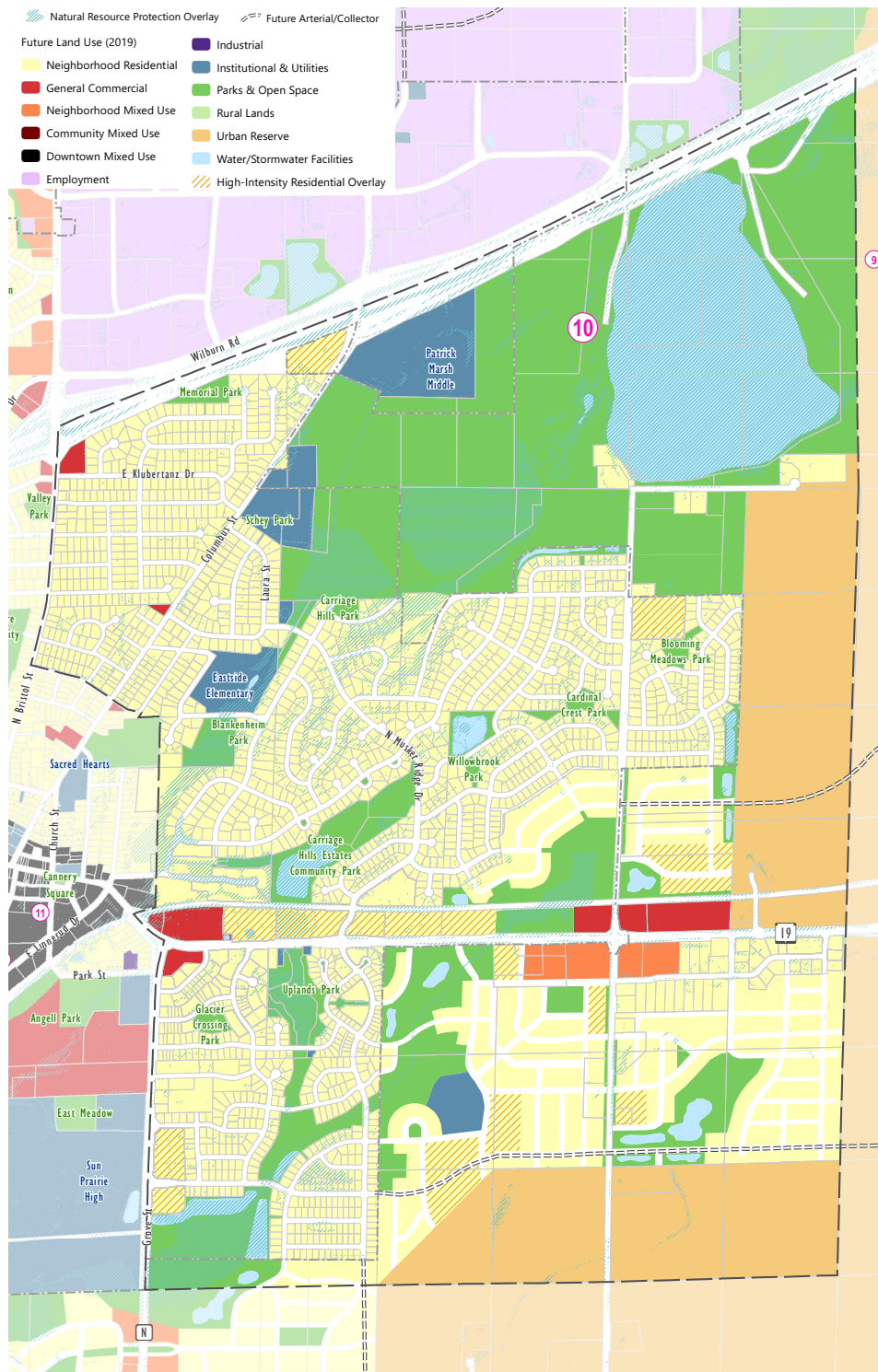
Figure 9-9: Central Planning Area (See Map 9-9 in Appendix D)



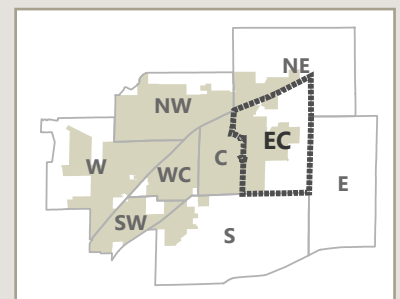
FUTURE LAND USE NOTES

- 11 See the Downtown Plan for more detailed vision for the DMU district.
- 12 The public works campus is a high-priority redevelopment site when the campus moves outside the downtown.
- 13A 13B The buildings destroyed by the 2018 explosion near Bristol and Main are high-priority redevelopment sites. See the Main/Bristol Street Visioning and Redevelopment Plan (2020).
- 14 Properties fronting Main St. from North St. to Union St. are within a Live-Work Overlay (LWO) District that is intended to preserve the residential character of the area as a transition to the downtown. This area should be evaluated as part of a detailed urban design planning process to determine if this development format should be maintained or allowed to change.
- 15 There are three plans that address desired uses, development types and overall character along this section of Main St. - Ruby Ln to Jones St: Central Main Street Corridor Redevelopment Plan (2018); Jones St to Bristol St: Bristol/Main Redevelopment Plan (2020); Bristol St. to Linnerud/Dewey: Downtown Plan.
- 16 Designation of Community Mixed Use is intended to supersede the Central Main Street Corridor Redevelopment Plan where that plan identifies commercial along the north side of Commercial Avenue.
- 17 The City will consider by plan amendment a proposal for residential in the area surrounding Craftsman Way and Bird Street.
- 18 Properties along Main Street from Clamar Road to Union Street are in a Main Street Overlay District (MSO) that includes site and building design guidelines for future redevelopment and building expansion projects along this corridor.

Figure 9-10: East Central Planning Area (See Map 9-10 in Appendix D)



East Central Planning Area



Planning Area Boundary: USH 151 to the north, half-way between Town Hall Drive and CTH VV to the east, the Meadow Crossing subdivision to the south, and Grove and Bristol Streets to the west.

Existing Land Use(s): Housing, agriculture, and open space.

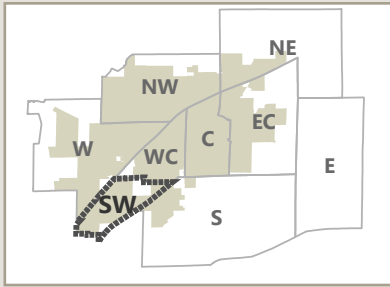
FUTURE LAND USE NOTES

- 9 This area provides an opportunity for future residential lots with water views, but is not recommended for development before 2040.
- 10 Consider public facilities that allows better access and usability of Patrick Marsh. Possible location for a Public/Private Event Venue or (Multi-) Cultural Center.

Detailed Concept (2009)

The Near Eastside conceptual plan includes undeveloped lands in the southeast section of this planning area (as well as portions of the South planning area). As illustrated, it suggests primarily residential uses with most of the area dedicated to single-family housing with some mix of small multi-unit buildings. There are a few areas identified as preferred locations for high-intensity multi-unit residential (HIR overlay). A full mix of residential neighborhood types are encouraged within this area. As shown, TND/hybrid neighborhoods are envisioned in the center of this district with conventional/conservation development to the east. There is a neighborhood mixed use and commercial node along WIS 19 and Town Hall Road.

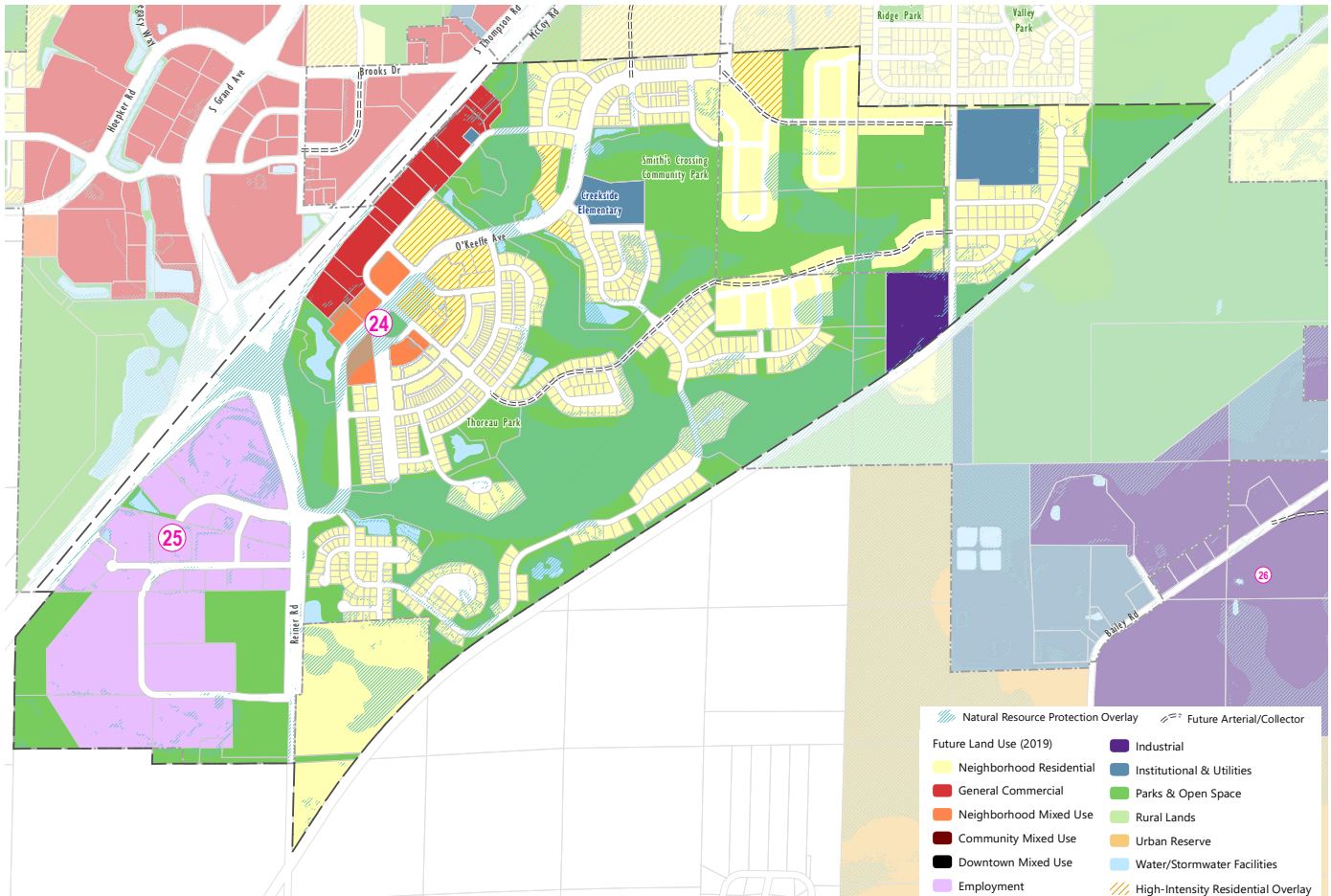
Southwest Planning Area



Planning Area Boundary: O’Keeffe Avenue and Sapphire Way to the north, the railroad on the east side, Waste Management - Madison Prairie Landfill to the south and USH 151 to the west.

Existing Land Use(s): Mixed residential housing, park and open space, and industrial.

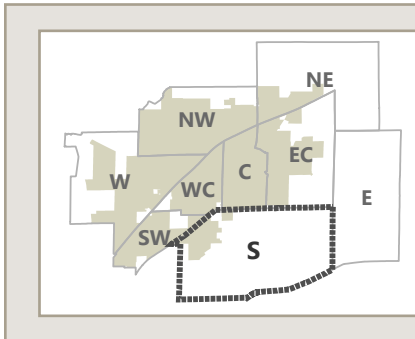
Figure 9-11: Southwest Planning Area (See Map 9-11 in Appendix D)



FUTURE LAND USE NOTES

- 24** This Neighborhood Mixed Use district shall have ground-floor commercial uses consistent with the approved General Development Plan.
- 25** Potential redevelopment area.

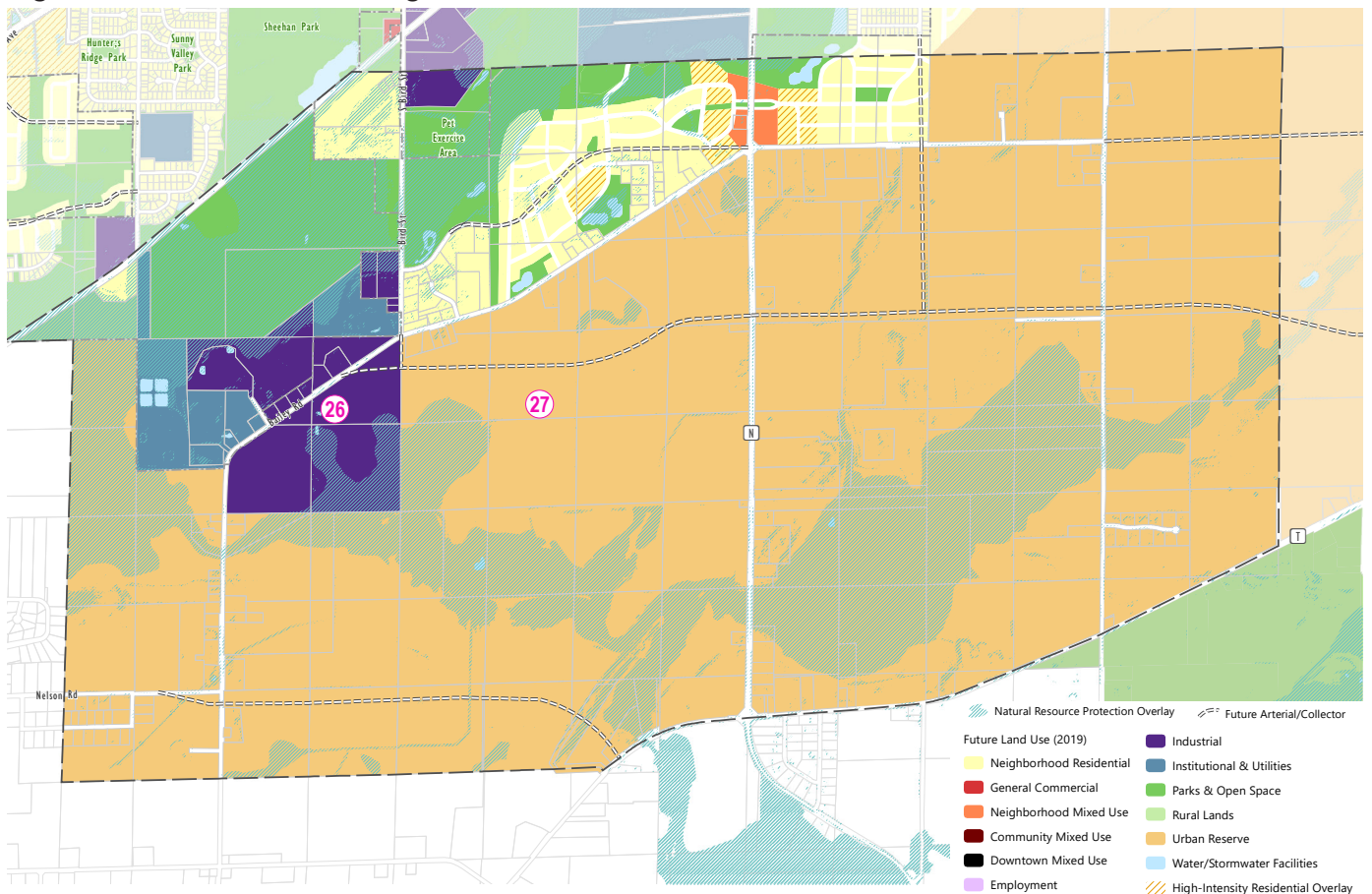
South Planning Area



Planning Area Boundary: Pet exercise area to the north, halfway between Town Hall Drive and CTH VV to the east, CTH T on the South, and Koshkonong Creek and the railroad tracks to the west.

Existing Land Use(s): Agriculture and single-family homes.

Figure 9-12: South Planning Area (See Map 9-12 in Appendix D)



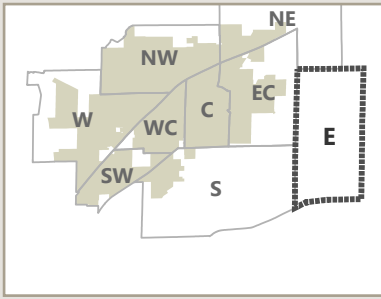
FUTURE LAND USE NOTES

- 26** Preferred location for urban industrial and heavy industrial businesses, especially those that are not a good fit for the business park.
- 27** Protect a route for a future far east side sewer interceptor.

Detailed Concept (2009)

The Near Eastside conceptual plan includes undeveloped lands in the northern section of this planning area (as well as portions of the East Central planning area). As illustrated, it suggests primarily residential uses with most of the area dedicated to single-family housing with some mix of small multi-unit buildings. There are a few areas identified as preferred locations for high-intensity multi-unit residential (HIR overlay). A full mix of residential neighborhood types are encouraged within this area. As shown, TND/hybrid neighborhoods are envisioned in the center of this district with conventional/conservation development to the west. There is a neighborhood mixed use node along CTH N north of Bailey Road.

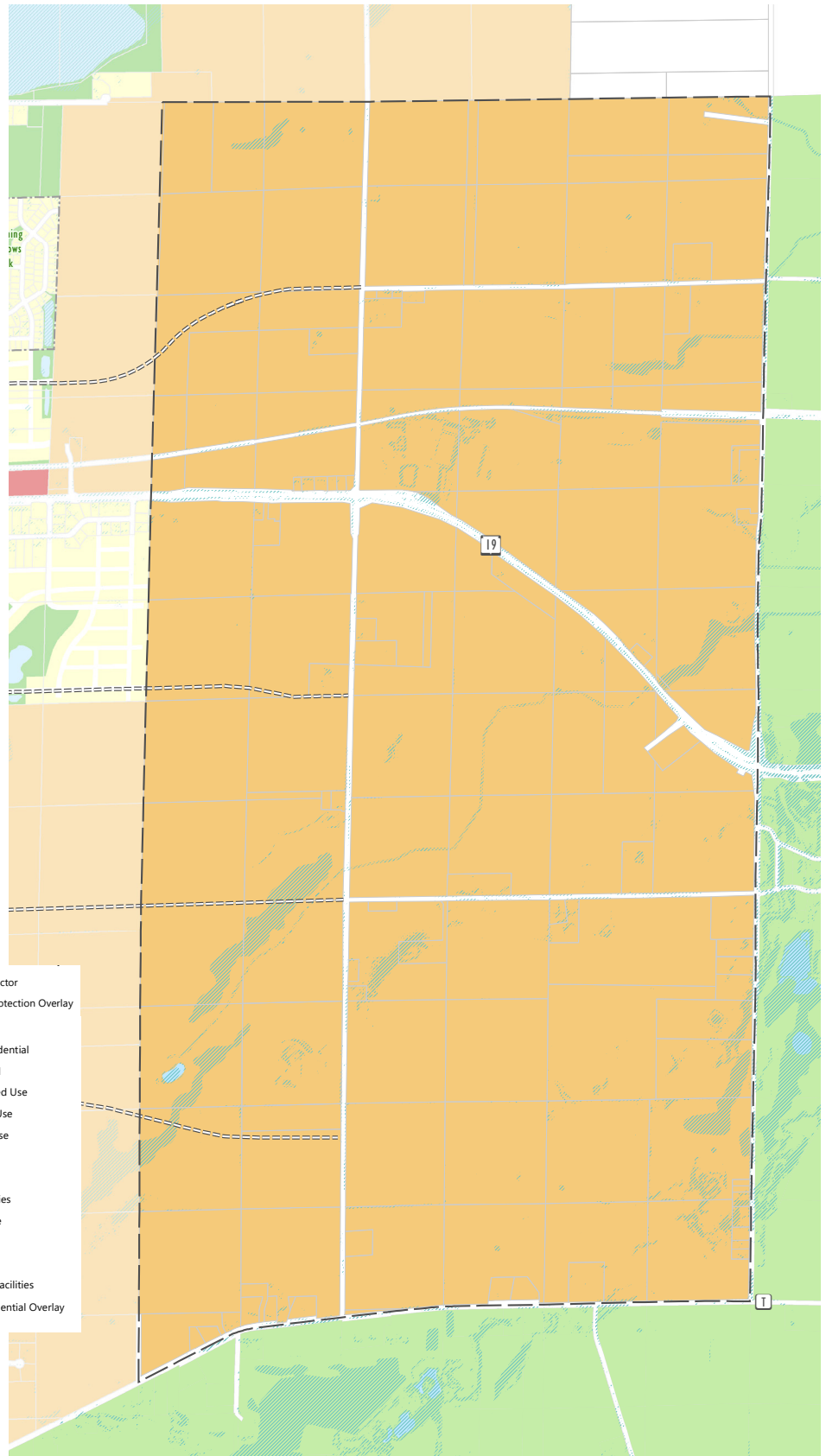
East Planning Area



Planning Area Boundary: USH 151 to the north, Twin Lane Road to the east, CTH T to the south, and between Town Hall Drive and CTH VV to the west.

Existing Land Use(s): Agriculture and rural housing uses.

Figure 9-13: East Planning Area (See Map 9-13 in Appendix D)



- Future Arterial/Collector
- Natural Resource Protection Overlay
- Future Land Use (2019)
 - Neighborhood Residential
 - General Commercial
 - Neighborhood Mixed Use
 - Community Mixed Use
 - Downtown Mixed Use
 - Employment
 - Industrial
 - Institutional & Utilities
 - Parks & Open Space
 - Rural Lands
 - Urban Reserve
 - Water/Stormwater Facilities
 - High-Intensity Residential Overlay