

5.1 OVERALL PLANNING CONCEPT

The West Main Street Corridor will occupy a crucial role in connecting the expanding Westside development with the rejuvenated Downtown. As the region develops, the corridor will no longer just be a connecting route from the Highway 151 interchange to downtown. The corridor will become a central hub repositioned to serve several new mixed-use developments as well as existing City neighborhoods.

The general pattern of development along the corridor should become more “urban” characterized by a change from the current pattern of one story buildings with large parking lots to a more compact form of

development that includes two- to four-story buildings, mixed uses, and improved site development to meet parking needs as well as improve pedestrian activation and accommodation. An “infill” strategy, guided by a regulating plan to provide coordinated development, can ensure that the West Main Street Corridor provides an integrated set of experiences between the new Westside development and the Downtown. The plan is based upon a few fundamental concepts that can be summarized as follows:

- Place buildings near the street edge to help define a consistent “street façade”

- Discourage large parking lots in front of buildings; place parking to the rear or side
- Locate active uses, such as retail and sidewalk cafes, along pedestrian routes.
- Create guidelines to encourage coordinated development and quality architectural character.

This chapter offers a description of each corridor zone and refers to an illustrative master plan drawing (fig. 5.1) which indicates a representative outcome of development as outlined in the regulating plan. The drawings are not meant to show a “required” outcome but to offer a visual example of desired patterns.

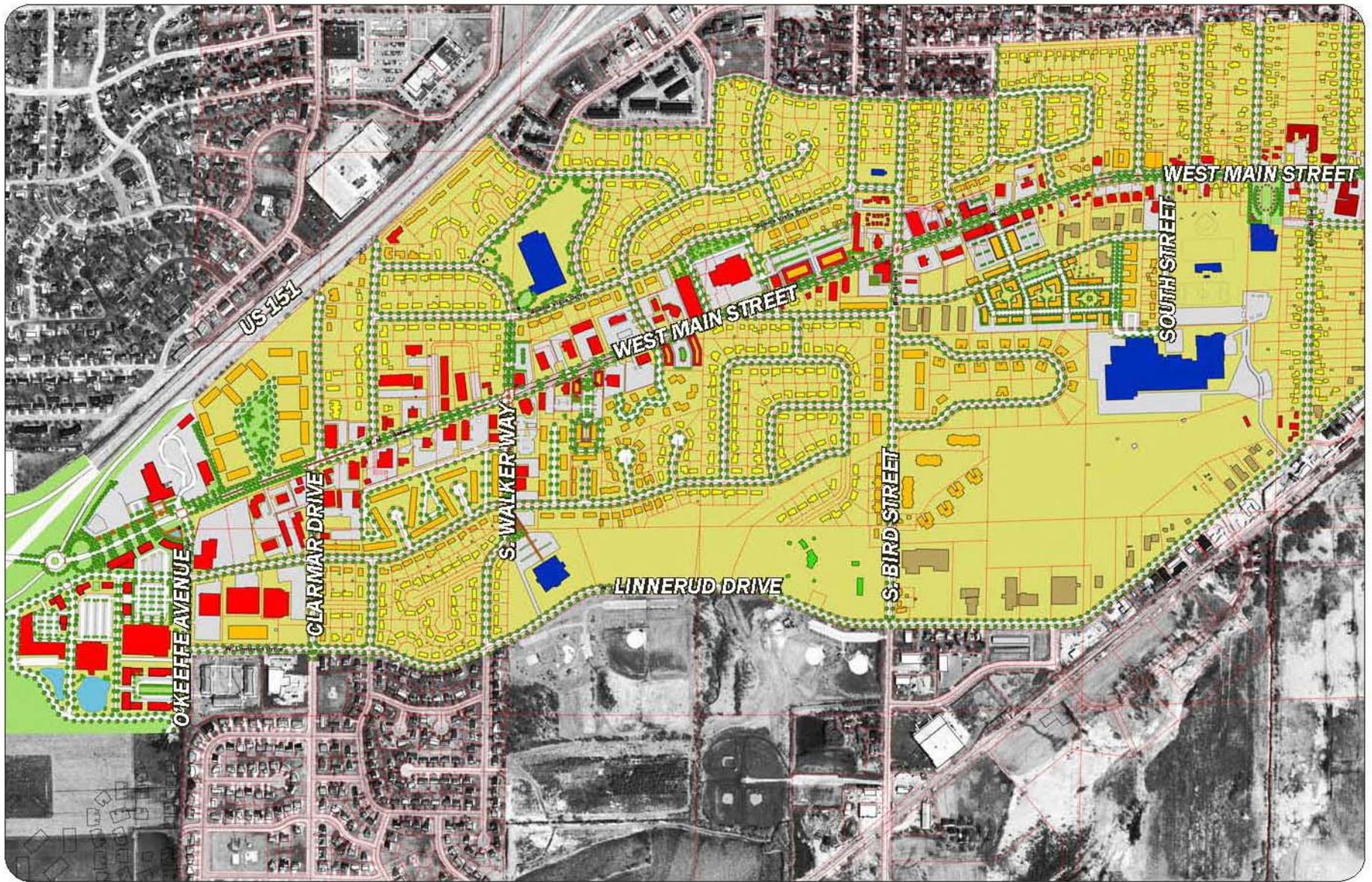


fig 5.1 Corridor Illustrative Plan: 1"=1000'

5.2 ROADWAY DESIGN : TYPICAL SECTIONS

West Main Street Road Design

The City of Sun Prairie is in the process of conducting traffic studies and planning the possible reconstruction of West Main Street. An engineering consultant will complete a traffic study with recommendations for specific improvements to improve traffic flow through the corridor. Some preliminary work has been started, but the City will delay completion of the engineering study until this corridor planning project is complete. This will allow roadway improvements to be coordinated and consistent with the recommendations made by this plan.

Critical Issues for Roadway Design

PDI met with MSA Engineering and City of Sun Prairie engineering and planning staff on July 29, 2005 to present some initial thoughts on the roadway design. Several critical issues regarding the roadway design and its impact on the corridor were outlined to guide the planning process. Critical considerations include:

- Maximize creation of economic value for the city

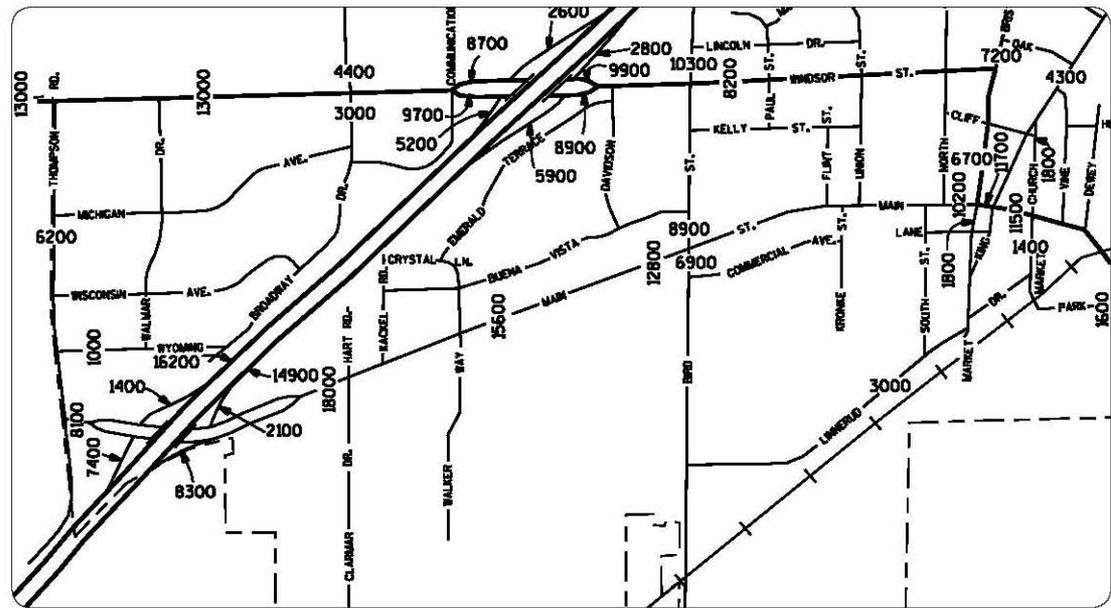


fig. 5.2 Average daily traffic counts (2002)

- Achieve a balance between vehicular, pedestrian, aesthetic and redevelopment needs within the corridor and for the community as a whole
- Develop an overall integrative strategy for the corridor while recognizing the need for specific planning initiatives that address each corridor district.
- Investigate potential for streetscape/landscape features to improve the street image and to encourage private investment along the corridor
- Create a more accommodating and attractive environment for pedestrians
- Improve circulation of vehicular access between parcels through shared drives: investigate possibilities for cross-parcel easements as redevelopment occurs
- Minimize impact of widening/road design on existing property
- Investigate infrastructure costs vs. benefits and impacts on property values

Roadway Design Concept

MSA has prepared several potential options for roadway cross sections and lane configurations ranging from one vehicular lane in each direction to options with two vehicular lanes in each direction. All options feature a shared left turning lane down the middle of the road for the length of the corridor. Based on those options, which are only preliminary, the following observations and recommendations are made:

- The roadway cross section design should not be one uniform design across the entire length of the corridor, but should adapt to the conditions present in specific segments. For example, an option with four driving lanes is probably appropriate from the Highway 151 interchange to Bird Street. A two driving lane design can then lead into

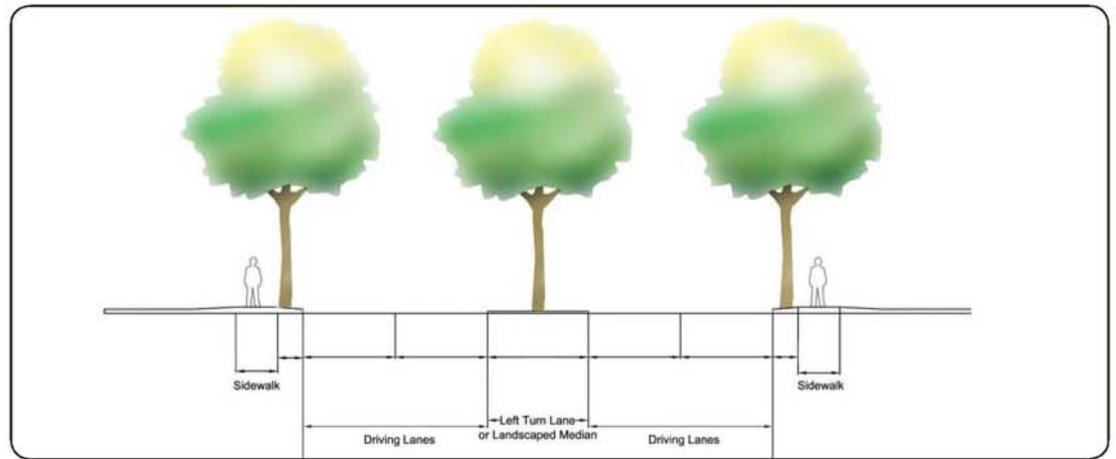


fig. 5.4 Potential Main Street road concept: section

the downtown, reflecting the “main street” character of this portion of the corridor.

- Land use and planning policies should encourage the elimination of as many curb cuts as possible along the corridor over time.

Figures 5.3 and 5.4 illustrate an idea that should be investigated as the detailed engineering of the street proceeds. The

figures illustrate the notion of adding amenities to the center turning lane (colored asphalt, colored concrete, or pavers) broken at intervals with a landscaped median where allowable. The medians would be installed where left turn movements were not required or appropriate. Crosswalks should be marked by the same special pavement material. Additional landscape elements would be provided between the curb and the sidewalk depending on the width of the right-of-way. The feasibility of providing any or all of these amenities will depend on many factors yet to be determined including the total number of lanes; the width of the right-of-way; and the locations and number of curb cuts (driveways) along the corridor.

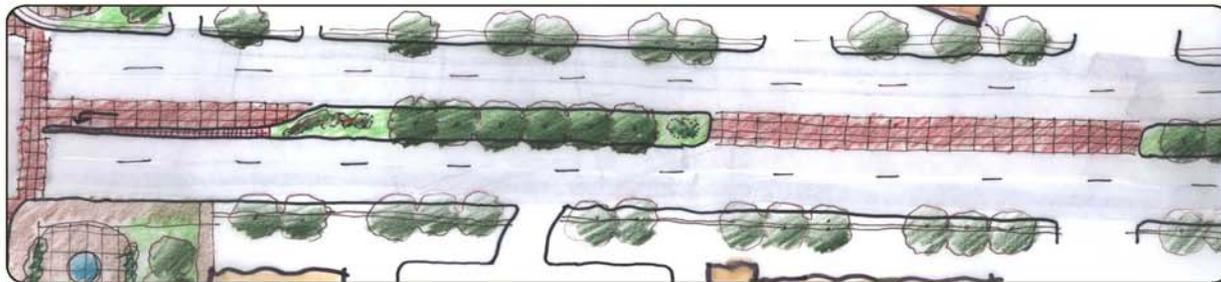


fig. 5.3 Potential Main Street road concept: plan

5.3 WEST ILLUSTRATIVE PLAN

The West Study Area presents a unique set of challenges and opportunities. A key objective for this area will be to establish a gateway entrance into Sun Prairie through building placement, facade improvements, and landscape development. In addition, traffic movement patterns should be revised to create more access points and circulation routes through the area. For example, a more traditional city block and street pattern is proposed for the current “big box” development. The plan drawing for the West Study Area (fig. 5.5) outlines a number of key concepts and recommendations. The numbers are explained on pages 30-31.

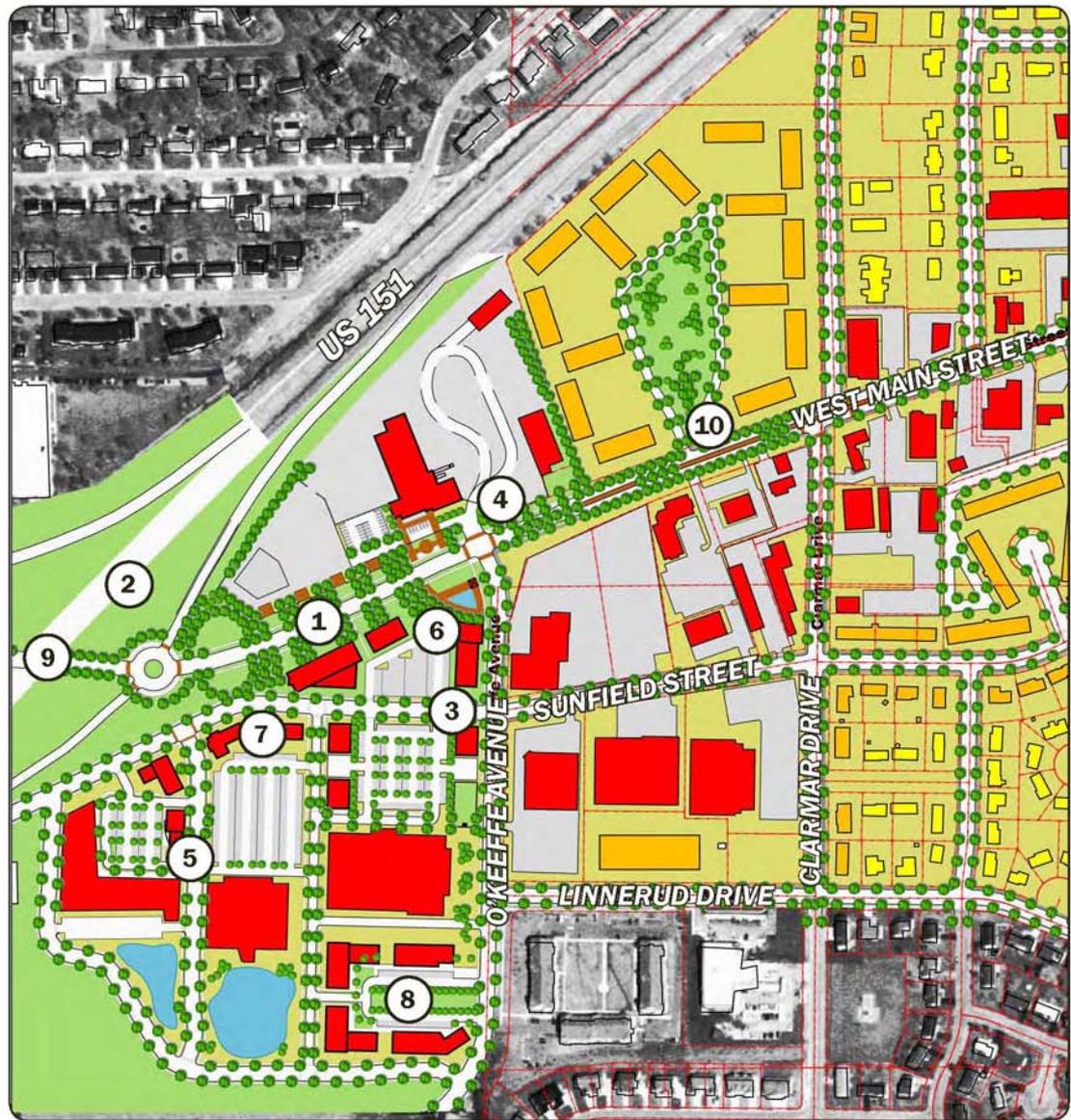
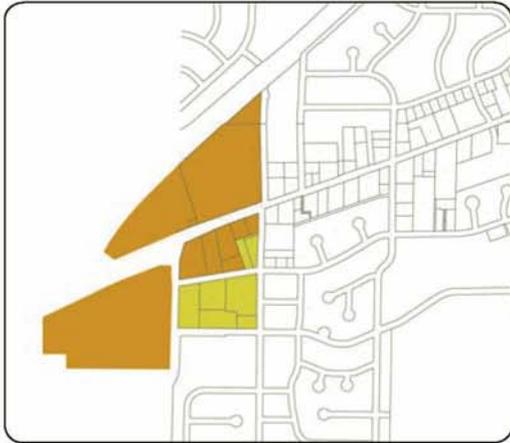


fig. 5.5 West Illustrative Plan: numbers are explained on pages 30-31.

1. Gateway Landscape/Streetscape: Create tree massings with new coordinated landscape in both the right-of-way (fig. 5.8) and along private lot edges. In addition, pedestrian-friendly features should be used, such as decorative fencing, lighting, and active building edges where possible.

2. Controlled Intersection/roundabout at West Main Street and Highway 151 off-ramp: Changes should be considered to create more of an intersection condition

at the Highway 151 off ramp and West Main Street. A roundabout is an option which should be considered.

3. Re-route McCoy Road to Intersect with Sunfield Street: McCoy Road should be re-aligned to intersect with O’Keeffe Avenue at Sunfield Street. This will move the intersection further to the south in order to create a clear entrance point for the “big box” area as well as free up the southwest corner of O’Keeffe Avenue and Main Street for redevelopment.

4. Brooks Tractor renovations: A new building proposal is under consideration by Brooks Tractor. This proposal offers an opportunity to move the building closer to the street and use the display areas for equipment as integral components of the gateway landscape.

5. Create a street/block system for “big box” area: As the area changes over time, extend a street grid into the development zone to provide a coherent pattern that

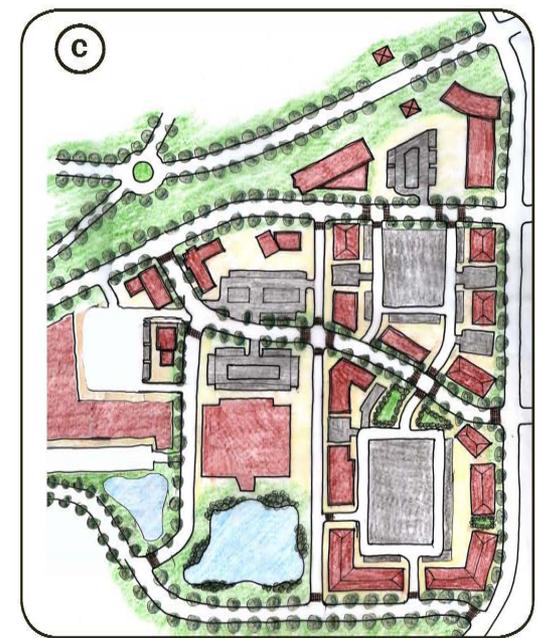
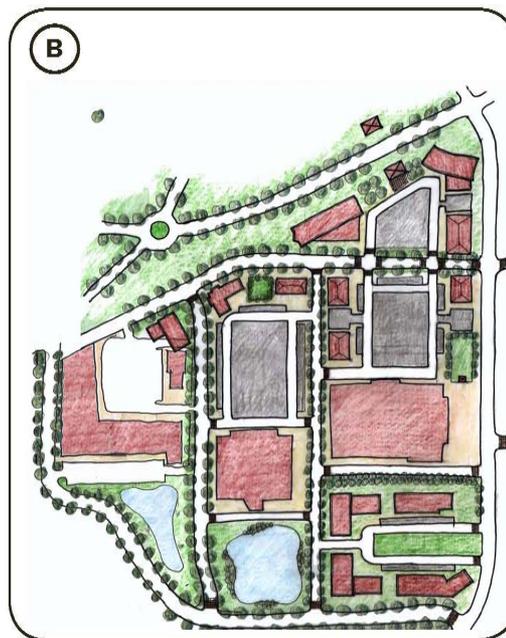
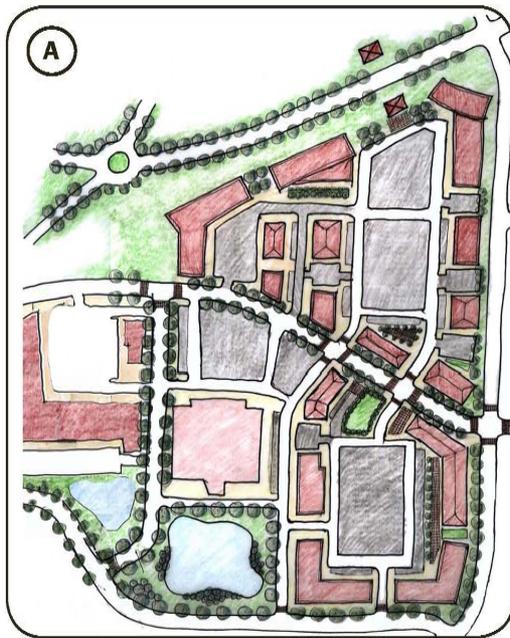


fig. 5.6 Creating a street/block system for the “Big Box” area allows for development flexibility as indicated in these three scenarios. Alternatives A and C include an extension of Linnerud Drive westward to link with McCoy Road. The management of stormwater runoff may require unique or unconventional solutions, such as underground storage under paved parking lots.



fig. 5.7 Proposed character of development, Main and O’Keeffe Streets

incorporates structures, access points, circulation flow, required parking, and pedestrian routes (fig 5.6).

6. Develop new retail at O’Keeffe Avenue and West Main Street: Re-routing the McCoy Road/O’Keeffe intersection to the south will give rise to a parcel of land which could become a gateway commercial development along the south



fig. 5.8 Proposed “Gateway” character along West Main Street

side of West Main Street (fig 5.7). A public plaza is proposed at the corner that functions as a gateway feature.

7. “Liner buildings” to create “town square” character: Infill development is proposed around the current “big box” stores, creating a more inviting and pedestrian-oriented character to the area (fig 5.9).

8. Neighborhood mixed-use center: Opportunities exist for adding residential uses to the area, in the form of housing on the second and third floors above ground floor retail.

9. Landscape/streetscape improvements under the Highway 151 bridge: This area will become an important connection point between the current West Main Street Corridor and the future



fig. 5.9 Proposed pedestrian character

Westside core commercial development. To integrate the areas, design the street approaches and the underside of the bridge to become part of the overall streetscape character so that there is no “gap” between the two corridors.

10. West Main Street road design and streetscape: Street reconstruction will focus on addressing traffic flow, turning movements; pedestrian accommodation and safety; and streetscape improvements. Access to existing businesses during construction will also be an important concern.

5.4 CENTRAL ILLUSTRATIVE PLAN

Concepts for the Central Study Area include incremental infill projects, as well as road and streetscape improvements that can serve to provide a more integrated image and character for the currently fragmented strip development. The plan drawing for the Central Study Area (fig. 5.10) outlines a number of key concepts and recommendations. The numbers are explained on page 33.

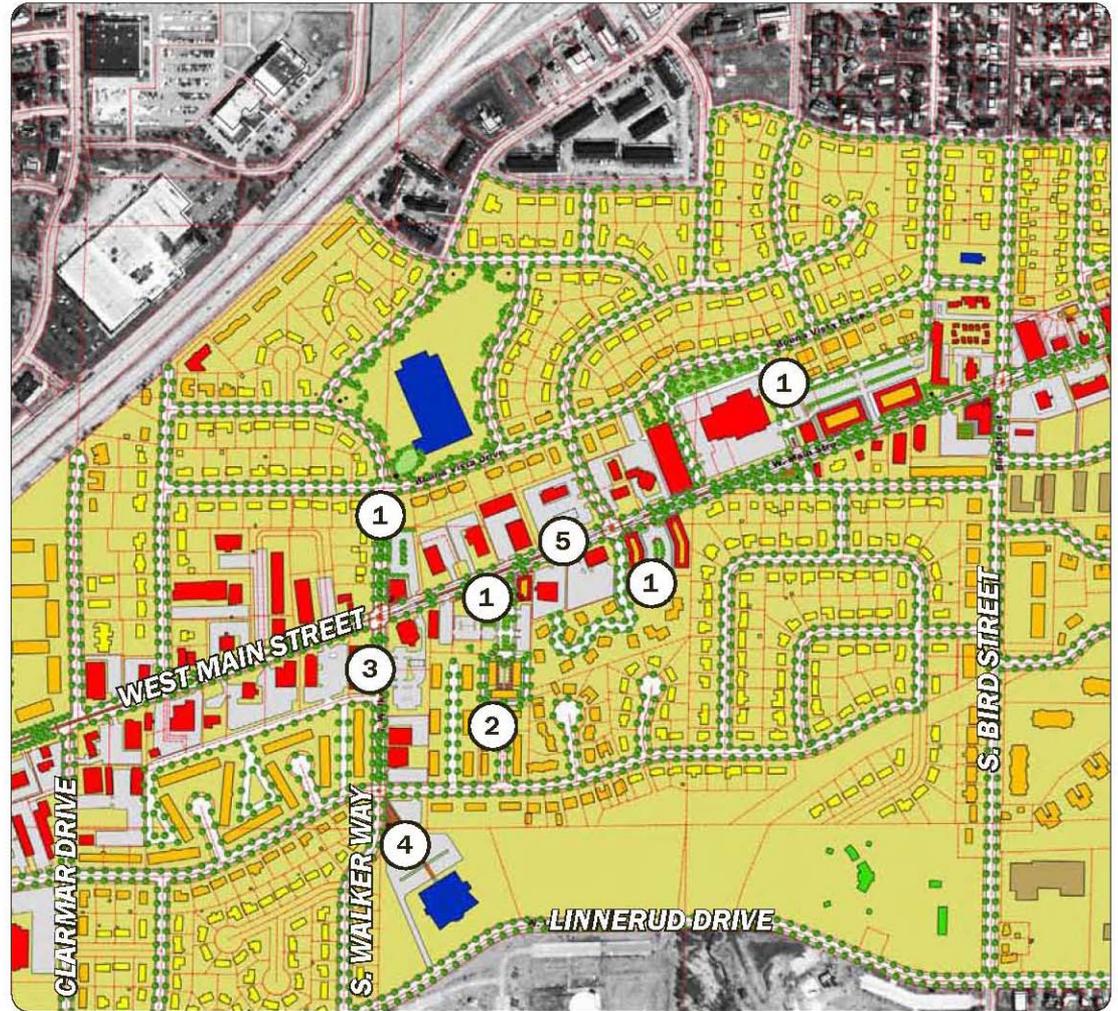


fig. 5.10 Central Illustrative Plan. The numbers are explained on page 33

1. Commercial Redevelopment Activity on “properties susceptible to change”: As redevelopment occurs along West Main Street, new development patterns as outlined in the regulating plan will replace the current auto-dependent strip commercial pattern (figs. 5.13 & 5.14). These patterns, in general, place buildings near the street edge, with parking located to the side or rear of the lot (fig 5.11 & fig 5.12).

2. Multifamily residential redevelopment activity on “properties susceptible to change”: Redevelopment on the deeper parcels may include residential uses to the rear of the property as commercial activity can be expected to be located closest to West Main Street. This change also offers an opportunity to utilize multifamily as a buffer between commercial activity and the existing single and multifamily uses that adjoin the corridor.

3. Boulevard street connection along Walker Way: A boulevard median is proposed along Walker Way between the Westside Elementary School and the Library (figs. 5.15 - 5.18). This boulevard



fig. 5.11 Proposed mixed-use character in which building face defines the street edge

would create a civic axis and relate these two important public buildings to West Main Street and allow a more attractive pedestrian connection between the two structures. Pedestrian safety enhancements at the intersection, such as textured crosswalks, on-demand signal

timing to aid pedestrians, and additional hours of crossing guard assistance should be considered.

4. Library plaza: A small plaza is proposed at the southern terminus of the boulevard, connecting it to the tree-lined walkway (fig. 5.16) that extends north from the Library entrance through the parking lot.

5. West Main Street road design and streetscape: Street reconstruction will focus on addressing traffic flow, pedestrian accommodation and safety, turning movements, and streetscape improvements.



fig. 5.12 Parking located to side of retail lot



fig. 5.13 Existing strip commercial development-Main Street Shopping Center



fig. 5.14 Proposed mixed-use development



fig. 5.15 Proposed boulevard character : Walker Way



fig. 5.16 Library Plaza at boulevard terminus



fig. 5.17 Opportunity to connect Civic Buildings

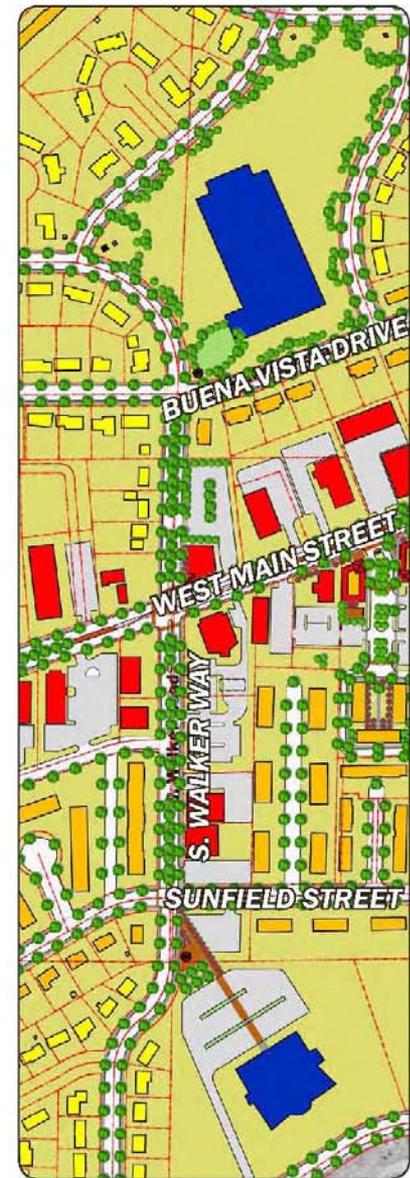


fig. 5.18 Boulevard connection : Walker Way

5.3 EAST ILLUSTRATIVE PLAN

Concepts for the East Study Area include incremental infill projects and road and streetscape improvements that can serve to enhance the commercial and residential “Main Street” character that exists. The plan drawing for the East Study Area (fig. 5.19) outlines a number of key concepts and recommendations. The numbers are explained on page 37.

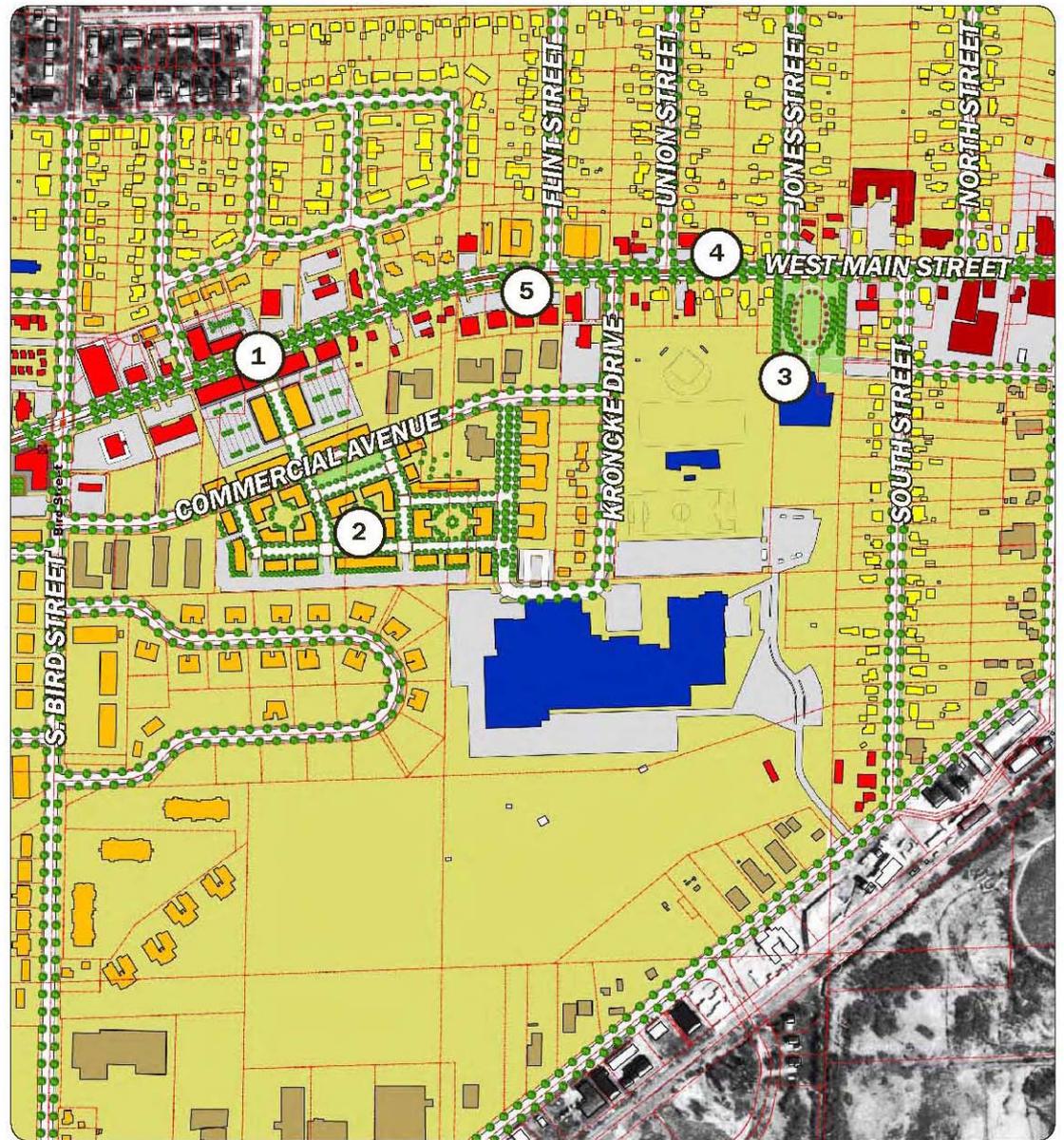


fig. 5.19 East Illustrative Plan. The numbers are explained on page 37

1. Commercial Redevelopment Activity on “properties susceptible to change”: As redevelopment occurs along West Main Street, new development patterns (fig. 5.20-22) as outlined in the regulating plan will replace the current auto-dependent strip commercial pattern. These patterns, in general, place buildings near the street edge, with parking located to the side or rear of lots.

2. Multifamily residential redevelopment activity on “properties susceptible to change”: Redevelopment on the deeper parcels may include residential uses to the rear of the property as successful commercial activity can be expected to be located closest to West Main Street.



fig. 5.21 Street-defining buildings

This strategy also offers an opportunity to utilize multifamily as a buffer between commercial activity and the existing single and multifamily uses that adjoin the corridor.

3. Greenspace Enhancements: Improvements for the existing green space in front of the YMCA are proposed, including removing the

fence; adding landscape elements; and retaining and enhancing the existing prairie restoration area to provide a more unified and noticeable image from Main Street; and better pedestrian access.



fig. 5.22 Potential mixed-use infill redevelopment



fig. 5.20 Proposed multifamily character

4. Preservation of “residential downtown” character: Guidelines are proposed to retain the residential scale of this area which provides a transition into the downtown.

5. West Main Street road design and streetscape: Street reconstruction will focus on addressing traffic flow, pedestrian accommodation and safety, turning movements, and streetscape improvements.

